



CONDITION SURVEY AND PAVER IMPLEMENTATION CANNON AIR FORCE BASE, NEW MEXICO

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William P. Grogan

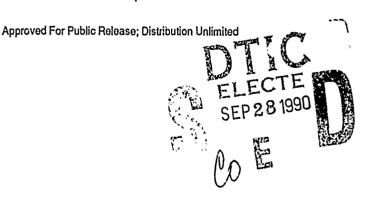
by

Geotechnical Laboratory

DEPARTMENT OF THE ARMY Waterways Experiment Station, Corps of Engineers 3909 Halls Ferry Road, Vicksburg, Mississippi 39180-6199



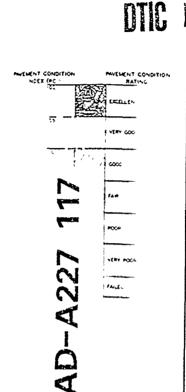
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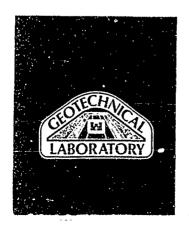
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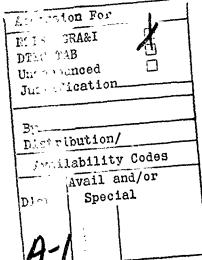
PREFACE

The condition survey described in this report was requested by Military Interdepartmental Purchase Request (MIPR) No. N589-119 dated 7 April 1989 from the Headquarters TAC/DEPF, Langley Air Force Base, VA, to the US Army Engineer Waterways Experiment Station (WES), Vicksburg, MS.

The condition survey at Cannon Air Force Base was performed by a WES condition survey team during the period 1 to 8 June 1989. The team consisted of Messrs. S. J. Alford, R. H. Barlow, Jr., W. P. Grogan, and D. D. Mathews, Pavement Systems Division (PSD), Geotechnical Laboratory (GL). This report was prepared by Mr. Grogan under the supervision of Mr. H. H. Ulery, Jr., Chief, PSD. The work was under the general supervision of Dr. W. F. Marcuson III, Chief, GL, WES. Ms. Odell F. Allen, Visual Production Center, Information Technology Laboratory, edited the report.

COL Larry B. Fulton, EN, was the Commander and Director during the preparation and publication of this report. Dr. Robert W. Whalin was the Technical Director.

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CONVERSION FACTORS, NON-SI TO SI (METRIC) UNITS OF MEASUREMENT

Non-SI units of measurement used in this report can be converted to SI (metric) units as follows:

<u>Multiply</u>	By	To Obtain
feet	0.3048	metres
inches	2.54	centimetres
square feet	0.09290304	square metres
square yards	0.8361274	square metres

CONDITION SURVEY AND PAVER IMPLEMENTATION CANNON AIR FORCE BASE, NEW MEXICO

PART I: INTRODUCTION

Background

This report describes the condition survey and initial implementation of a pavement management system using the PAVER system of the airfield pavements at Cannon Air Force Base (AFB), NM. The implementation was performed to provide base engineers with the initial data base required for making pavement management decisions concerning costs and maintenance requirements. The condition survey was performed by the US Army Engineer Waterways Experiment Station during the period 1 to 8 June 1989.

Objective and Scope

2. The overall objective of this project was to determine the pavement condition of the airfield pavements at Cannon AFB and to input the information into a PAVER data base to provide the base engineers with a permanent data base to use for future pavement management decisions. This objective was accomplished by:

- $\frac{a}{1}$. Performing a condition survey of the pavements in accordance with AFR 93-5,**
- b. Inputting the pavement network and condition survey information into PAVER to calculate a pavement condition index (PCI) of each of the pavement features.
- c. Completing the data base implementation by compiling pavement construction data and inputting the information into the PAVER data base Atom

Producing detail drawings of the pavement features to ensure that future condition surveys will be performed at the same locations as the one performed for this report.

^{*} Headquarters, Department of the Air Force. 1981 (May). "Airfield Pavement Evaluation Program," Air Force Regulation AFR 93-5, Washington, DC.

PART II: PAVEMENT CONDITION SURVEY

Introduction

3. An airfield pavement condition survey determines the present surface condition of the airfield pavements. The procedure used in performing the condition survey was developed by the US Army Corps of Engineers and has been accepted as a regulation by the US Air Force. Knowledge of the condition survey procedures discussed in AFR 93-5* is beneficial for the use and understanding of this report.

Pavement Definition and Identification

- 4. The pavement network is divided into three specific units in order to manage the pavement network effectively. The three units of division are the branch, the section, and the sample unit. The method for dividing the pavement network is detailed in AFR 93-5 and is briefly discussed herein.
- 5. Airfield pavement branches (or features) are defined by parameters such as the pavement type, construction history, and pavement usage. The branch designations used for Cannon AFB were established in the 1987 report "Airfield Pavement Evaluation, Cannon Air Force Base, New Mexico."** The branch designations, shown in Figure 1, are indicative of pavements subjected to similar traffic with like cross sections.
- 6. After each pavement branch has been defined, further division of the branch may be required for reasons such as traffic flow. Further division of branches is done into sections. For instance, a runway branch may be 150 ft† wide, but the majority of the traffic occurs in the middle of the branch. Therefore, a section is defined in the center of the branch with additional sections defined on either side of the middle section. The runways at Cannon AFB were divided into three sections. An apron may contain taxi lanes which

^{*} Headquarters, Department of the Air Force. 1981. "Airfield Pavement Evaluation Program," Air Force Regulation AFR 93-5, Washington, DC.

^{**} Air Force Engineering and Services Center. 1987 (May). "Airfield Pavement Evaluation Report, Cannon Air Force Base, New Mexico," Tyndall AFB, FL.

[†] A table of factors for converting non-SI units of measurement to SI (metric) units is presented on page 3.

the aircraft follow to their parking locations, a section which would differ from the areas used for the actual parking of the aircraft. Therefore, these elements of the branch may be divided into sections. If a branch requires no division, it is defined as containing one section.

- 7. After the pavement section definition has been completed, the section is divided into sample units. Sample units are conveniently sized areas of pavement on which the inspection is performed. A sample unit on asphaltic concrete (AC) pavement is a 5,000-sq ft area, and a sample unit on portland cement concrete (PCC) pavement consists of 20 slabs. A pavement section is divided into sample units for condition survey purposes only. Recognizing that not all sample units can be 5,000 sq ft or 20 slabs, deviations of 50 percent on either side of these values may be used for survey purposes.
- 8. When a section has been divided into sample units, it is ready to be surveyed. Inspection of all of the sample units within a section could require a considerable amount of time. Therefore, the random sampling method was developed to provide an adequate calculation of the PCI while inspecting only a portion of the sample units in a section. The method, further defined in AFR 93-5, allows for a reduction in the number of sample units surveyed without a significant loss of accuracy in the calculation of the PCI. It should be noted, however, that the inspection of all the sample units may be necessary for the estimation of maintenance and repair work.
- 9. An essential concept in pavement management is determining the deterioration of the pavement surface over time. The PCI is used in the PAVER system to determine this deterioration. Determining the PCI of a pavement section at different time intervals using the same sample units of the section in each survey gives a precise idea of the deterioration rate. Drawings of each pavement branch with sample unit locations illustrated are included in Figures 2 through 10. The locations of the sample units in the asphalt runway and taxiway branches were made using stationing. Referencing of the PCC branches including taxiways, aprons, and runways were made with respect to the location of the slabs in each branch. The sample units to be surveyed were located by counting slabs. The circled numbers indicate the sample units that were surveyed. The match lines on the drawings are keyed to the sheet number in the lower right hand corner of Figures 2 through 10.

Pavement Inspection

- 10. The performance of a condition survey consists of inspecting the pavement surface for various types of distresses, determining the severity of each distress found, and measuring the amount of distress within the sample unit. Distress quantities on AC pavement are measured in either linear feet or square feet within the sample unit. PCC pavement distresses are measured by counting the number of slabs affected within the sample unit.
- 11. The result of the condition survey is the PCI of the sample unit. The PCI is a value from 0 to 100 reflecting the condition of the pavement surface. The numerical index of the PCI ranges from 0-10, 11-25, 26-40, 41-55, 56-70, 71-85, and 86-100 corresponding to the PCI ratings of failed, very poor, poor, fair, good, very good, and excellent, respectively. The PCI is obtained by determining a deduct value for the amount and severity level of each distress type found in a sample unit. A correct deduct value is determined for the combined effect of various distresses on the pavement condition. The corrected deduct value is subtracted from 100, resulting in the PCI value. A sample unit with no distress has a PCI of 100 with varying amounts of distress decreasing the PCI value to a possible low of 0. The PCI of the pavement section is calculated by averaging the PCI's of the sample units surveyed in the respective section.
- 12. The majority of the pavement branches at Cannon AFB are rated from good to excellent conditions with some branches rated from very poor to fair. Figure 11 illustrates the condition ratings of the features at Cannon AFB. Photos 1 through 30 show various distresses that were observed on the airfield pavements.

PART III: PAVER DATA BASE IMPLEMENTATION

Introduction

- 13. The use of the PAVER system requires knowledge of both computers and the PAVER system. This report does not describe the operation of a computer; it does outline the necessary PAVER procedures in moderate detail. The "PAVER User's Guide"* goes into specific detail of all the procedures for setting up and using a PAVER data base and should be used as a reference when performing operations in the PAVER system.
- 14. The PAVER system consists of five different system functions. Performing each function requires the use of specific programs, files, and procedures. The five functions are data entry, system sign-on, data base update, report generation, and data analysis. Data entry, system sign-on, and data analysis do not directly interact with the PAVER data base, but data base update and report generation require data base interaction.

Data Entry

- 15. The pavement network data are entered into the PAVER data base in a logical order that defines the branches and sections first. The additional information that allows the user to perform data base related operations such as PCI calculation and report generation is then entered. The data must be in specific formats for it to be accepted by the data base. Three data input programs are used to prepare data for the specific formats: PAVERIN, EDITOR, and REFORMT. All of these programs have been written in the BASIC computer language and are operable on a personal computer that contains a BASIC system. The PAVERIN program is used to input the data into the correct formats; the EDITOR program is used for editing any errors that may have been placed in the data, and the REFORMT program is used to prepare the data for uploading onto the mainframe computer.
- 16. The condition survey data are collected in the field by recording the data manually on condition survey data sheets, or by inputting the data

^{*} Shahin, M. Y. 1985. "The PAVER User's Guide," ADP-356-1, US Army Construction Engineering Research Laboratory and US Army Facilities Engineering Support Agency.

directly into the FIELD program on a portable computer. The FIELD program places the data into PAVER format as the data are entered into the computer and saves the data in a file that can be directly uploaded to the mainframe computer. If the data are collected manually on survey sheets, the PAVERIN data input program is used to enter the data in the PAVER system. The data for Cannon AFB were collected and compiled on data sheets at the time of the survey and later input into PAVERIN.

17. The data for physical properties and construction history of the pavements at Cannon AFB were obtained from the 1987 evaluation report and from base engineering personnel. The physical property data were entered into the data base.

System Sign-On

18. The mainframe PAVER system currently resides on a Control Data Corporation (CDC) computer and is accessible through a remote terminal via a telephone link. The telephone link is achieved by using a modem and appropriate communication software. Connection to the system requires dialing the CDC computer for connection and then entering the appropriate access codes to sign-on the computer. The access codes (user ID, password, and charge number) are obtained when a charge account has been set up with CDC.

Data Upload and Data Base Update

19. Data are added to the data base either interactively or by using the BATCH method. The interactive method is used when the user is on-line to the CDC computer. This method is easier to perform but is more expensive. The BATCH involves transferring the data file created with the PAVERIN or FIELD programs from the personal computer to the CDC mainframe. Using either operation involves creating the file DATAFL on the CDC computer from which the data are read into the data base. After DATAFL is prepared, the PAVER system checks it for errors, and after corrections have been made, the data are loaded into the data base.

Report Generation and Data Analysis

- 20. The PAVER system generates reports that provide a summary or specific information based on the data stored in the mainframe data base. It also calculates information such as budget needs from data and analysis programs provided by PAVER. These reports can be generated either interactively or through a BATCH process. The BATCH process produces the report when the user is not signed-on to the CDC computer and is more cost-effective when generating large amounts of information. The interactive process, performed while the user is signed-on, can be used effectively when generating smaller reports and detecting data base errors.
- 21. There are two types of data analysis programs in the PAVER system: those that access the data base and those that do not access the data base. The difference in the two types is that the data base must be on line for the report to operate. The user responds to questions that the program asks, and then analysis results are produced based on those responses. The analysis reports can only be generated using the interactive process.
- 22. The data reports and analysis programs provide an engineer with the information required to make pavement management decisions. Those reports (LIST, INV, PCI, SCHED, and SAMPCUR) are found in Appendix A.
- 23. The LIST report lists the branch number, branch name, and number of sections in each branch. The INV report provides inventory information of each pavement section including surface type, branch use, pavement rank, and section area in square yards. The PCI report provides a list of the PCI's of each section ranked by PCI from low to high. The report SCHED gives a schedule of sections to be inspected in the next 5 years. The SAMPCUR report lists the summary and sample unit PCI and distress information on each pavement section in the data base.

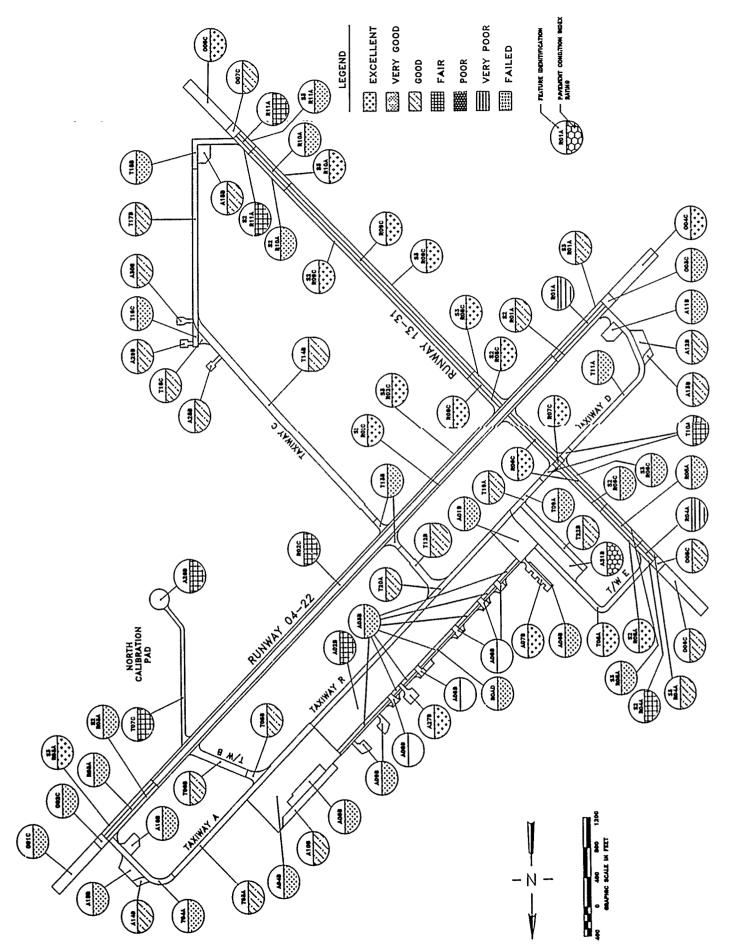


Figure 1. Branch identification at Cannon AFB

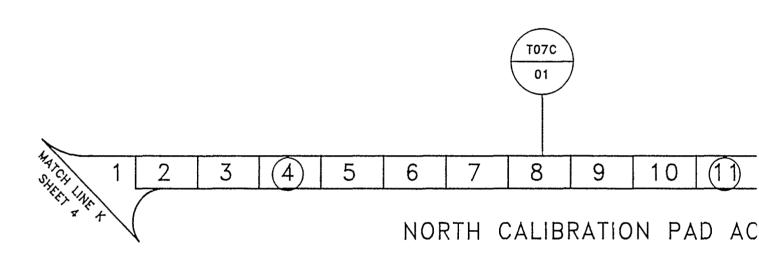
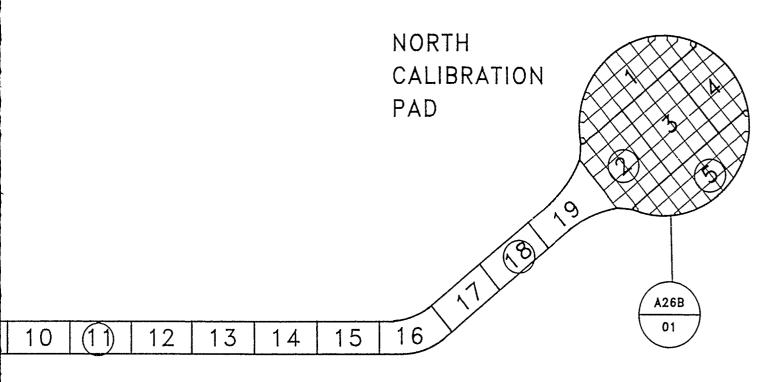
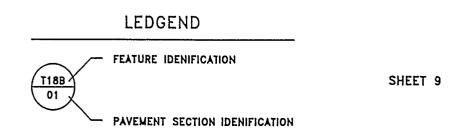


Figure 2. Sample unit location A01B, A12B, A



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Sample unit locations on Branches R01A, R02C, T11A, A01B, A12B, A13B, O03C, and O04C

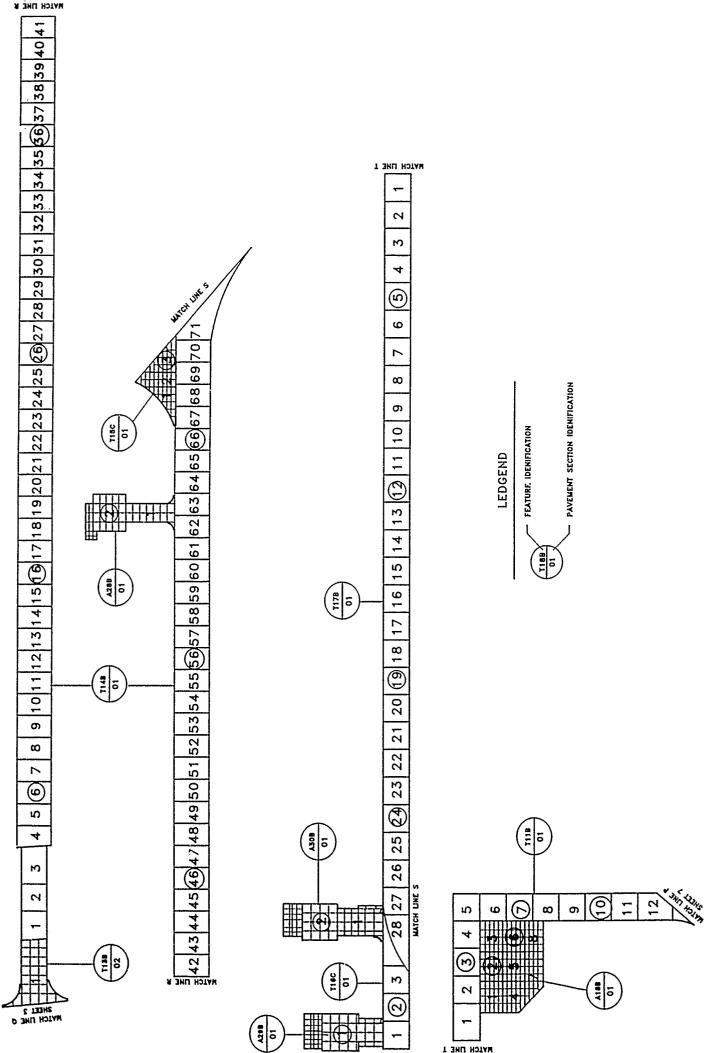


Figure 3. Sample unit locations on Branches R02C, R06C, R07C, T09A, T10A, T11A, T19A, T20A, T22A, A01B, A02B, A03B, A08B, A31B, and ROAD

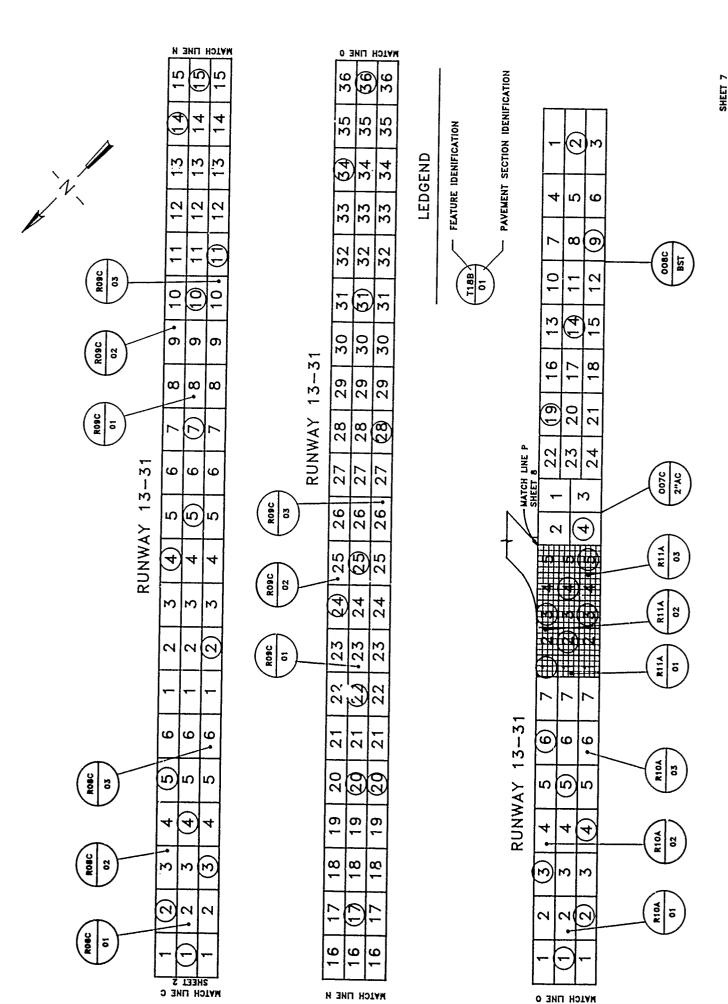
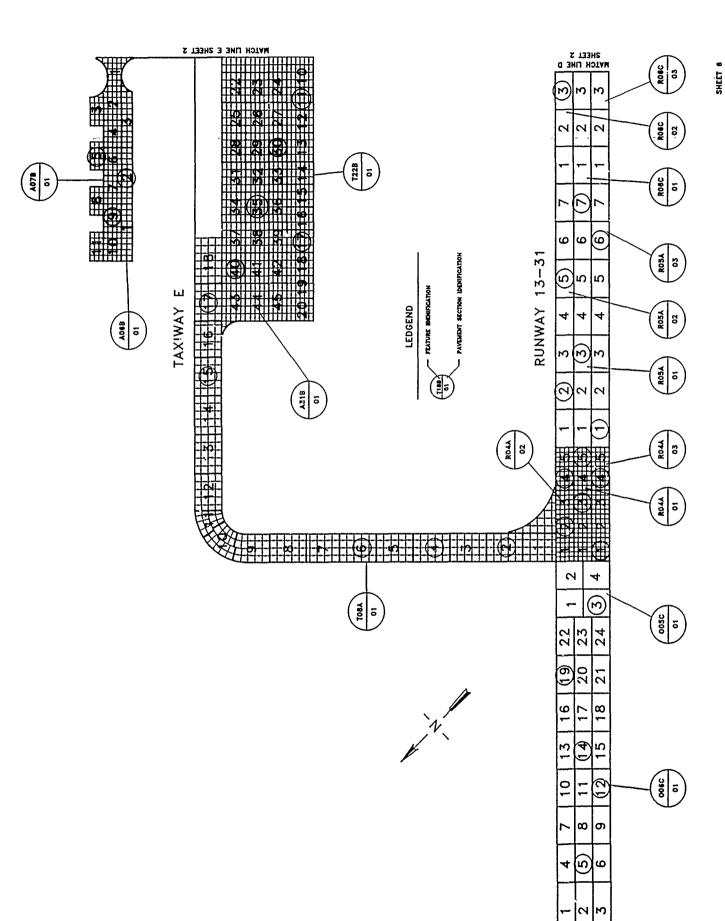
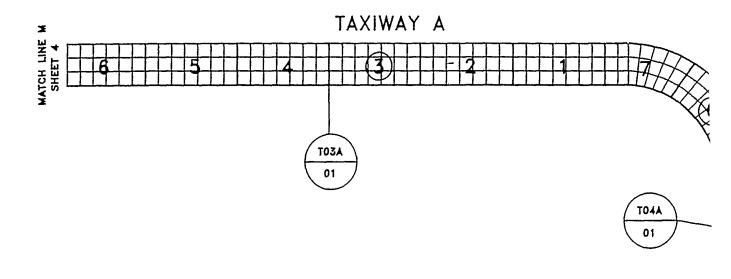


Figure 4. Sample unit locations on Branches R02C, T12B, T13B, T20A, A02B, A03B, A08B, A09B, A27B, and ROAD



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Figure 5. Sample unit locations on Branches R02C, T03A, T05B, T06B, T20A, A62B, A03B, A04B, A05B, A09B, A10B, and ROAD



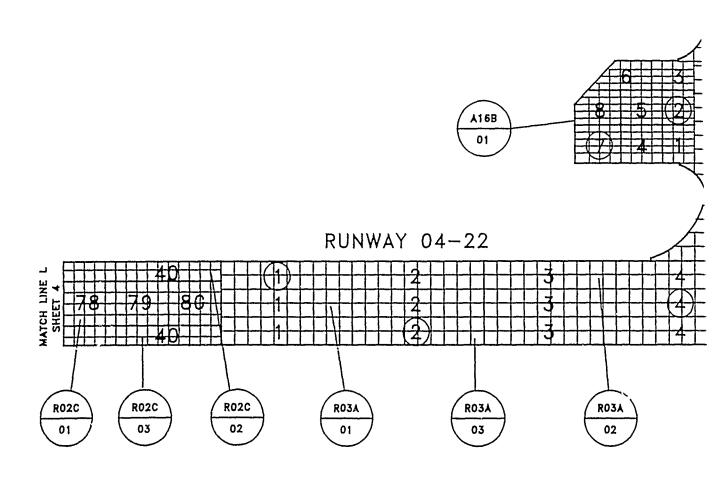
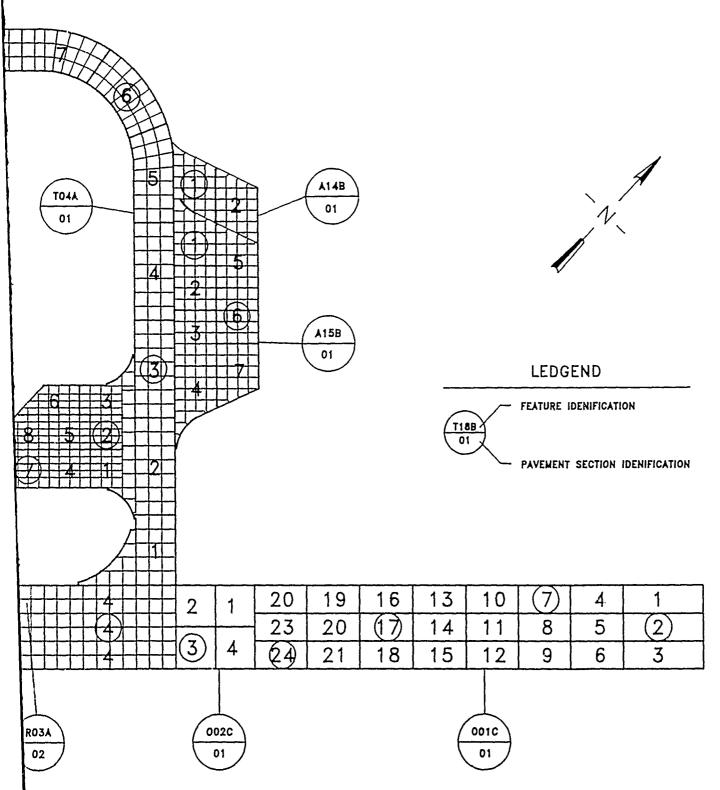


Figure 6. Sample unit T04A, A14F



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e 6. Sample unit locations on Branches R02C, R03A, T03A, T04A, A14B, A15B, A15B, O01C, and O02C

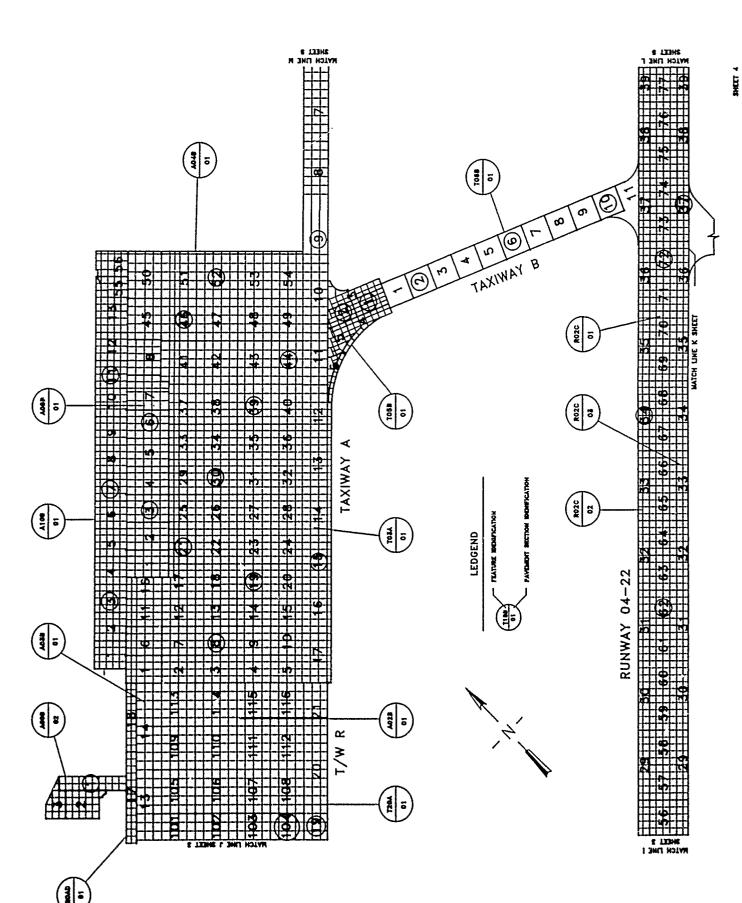
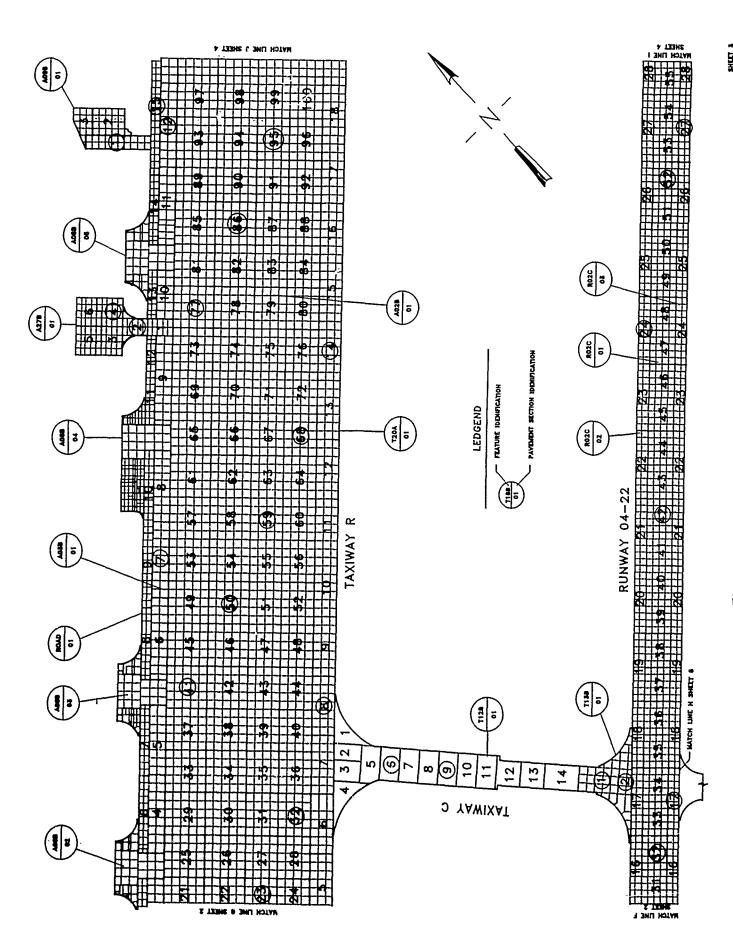
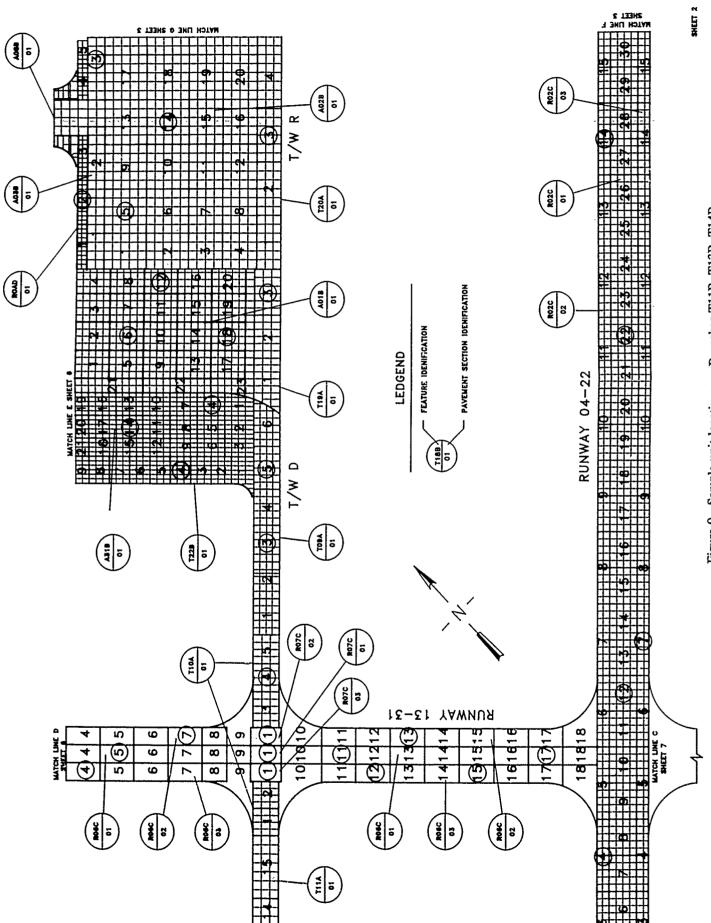


Figure 7. Sample unit locations on Branches R04A, R05A, R06C, T08A, T22B, A06B, A07B, A31B, O05C, and O06C





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Figure 9. Sample unit locations on Branches T11B, T13B, T14B, T15C, T16C, T17B, A18B, A28B, A29B, and A30B

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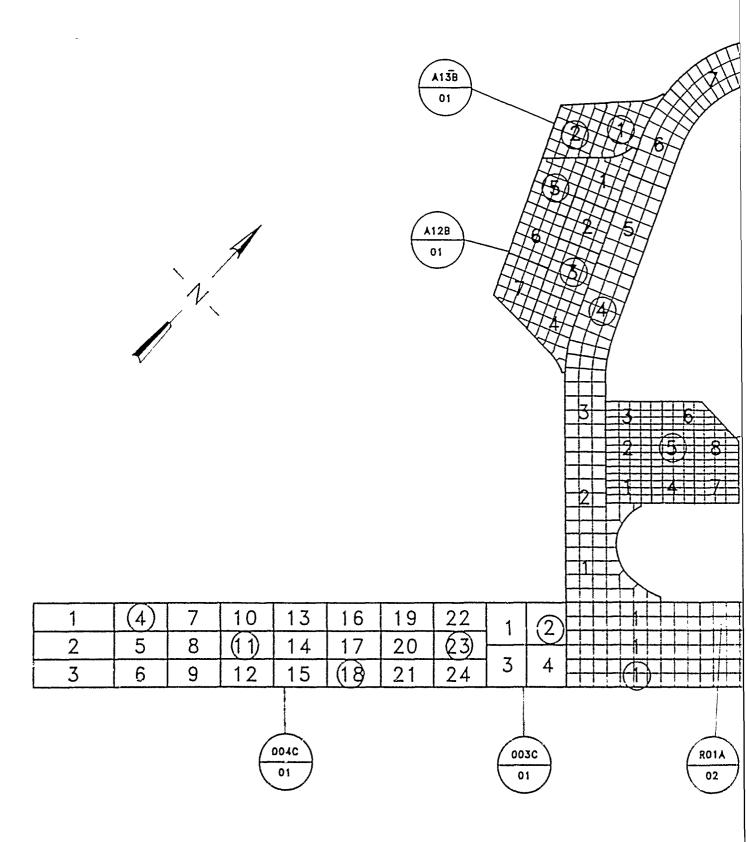
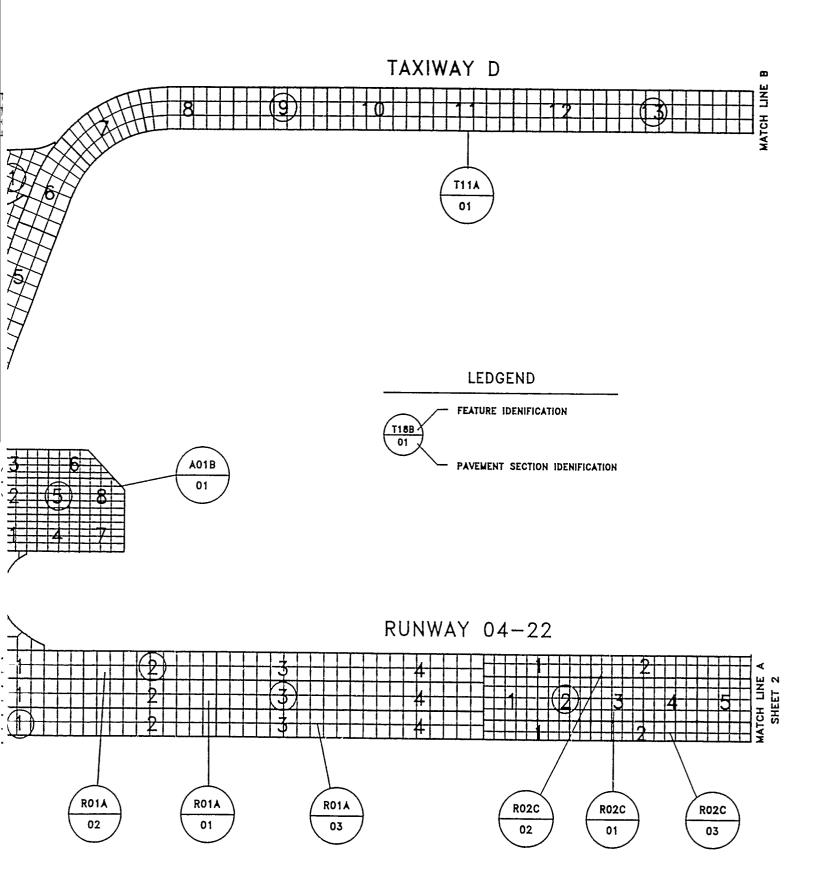


Figure 10. Sample unit



, 2 10. Sample unit locations on Branches T07C and A26B

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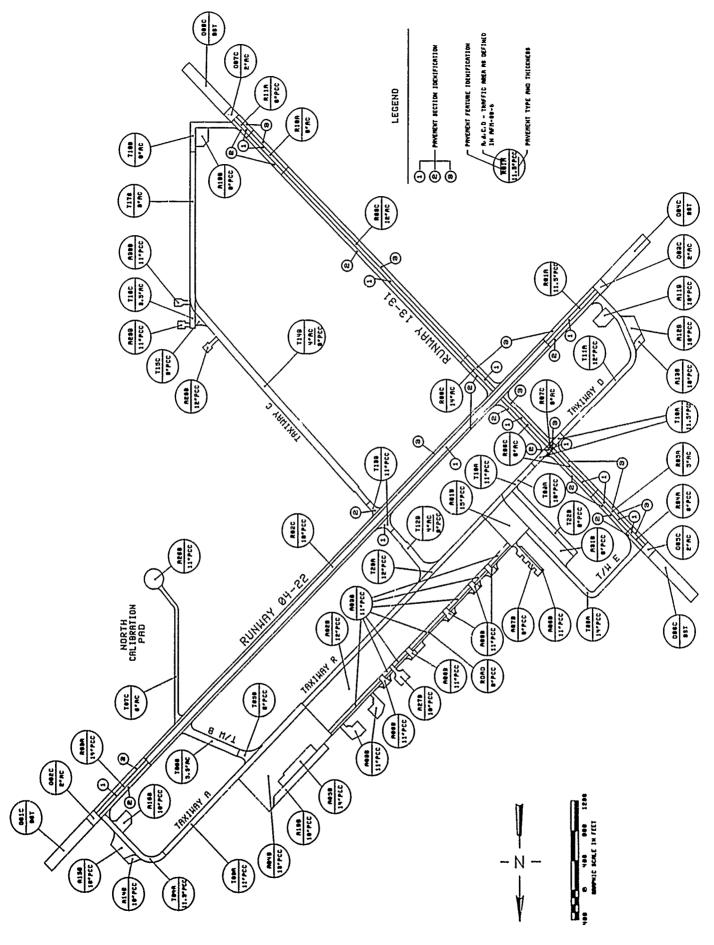


Figure 11. Condition rating of the branches at Cannon AFB



Photo 1. Overall view of RO1A



Photo 2. Small Patch on ROlA

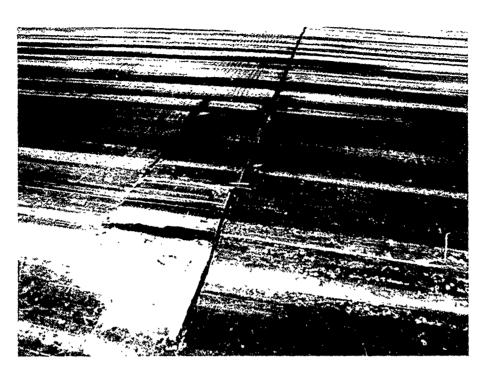


Photo 3. Large patch on R02C

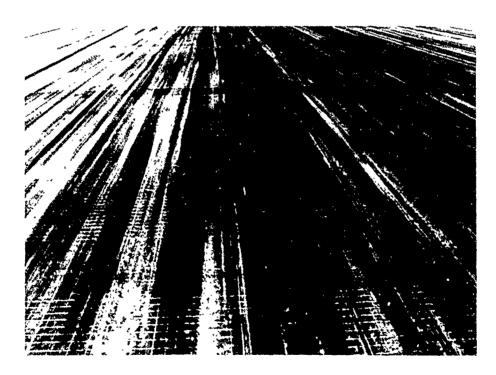


Photo 4. Center-line view of RO2C



Photo 5. Center-line view of RO3A



Photo 6. Overall view of R11A



Photo 7. Sealed longitudinal/transverse crack on R11A $\,$



Photo 8. Overall view of R10A

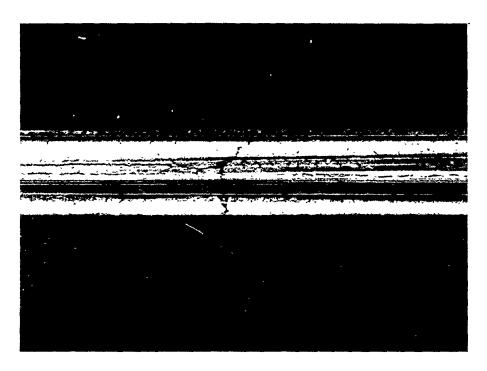


Photo 9. Longitudinal/transverse crack on R10A $\,$



Photo 10. Low-severity transverse crack on R10A

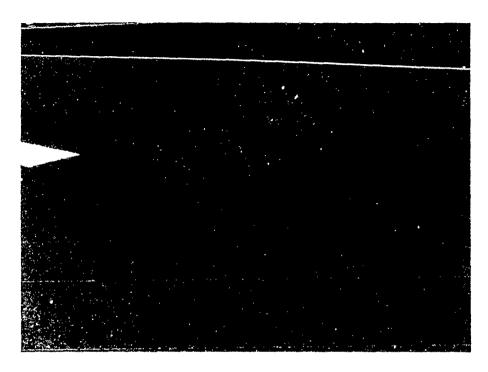


Photo 11. No distress on R9C

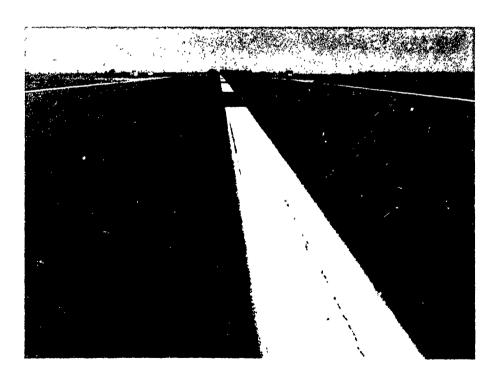


Photo 12. Overall view of R9C



Photo 13. Overall view of R4A

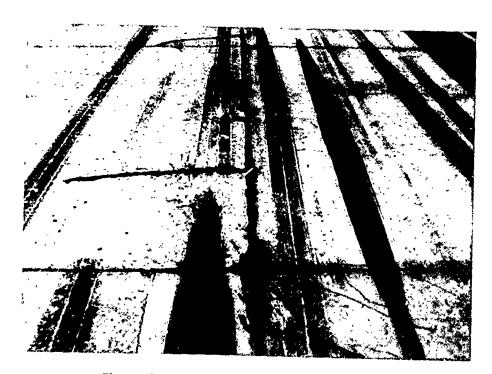


Photo 14. Failing spall patch on R4A



Photo 15. Sealed longitudinal crack on R4A

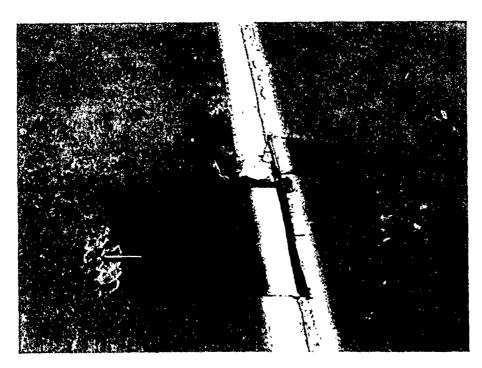


Photo 16. Patched corners on ROAD

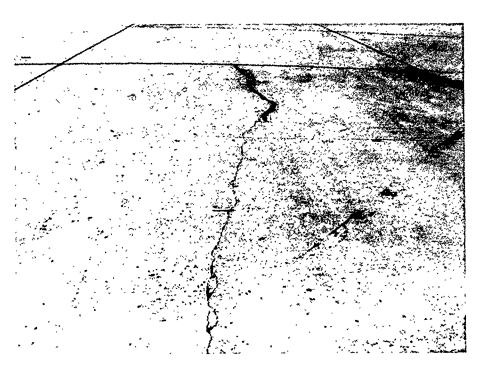


Photo 17. Longitudinal/transverse crack on A02B



Photo 18. Sealed longitudinal/transverse crack on T3A $\,$

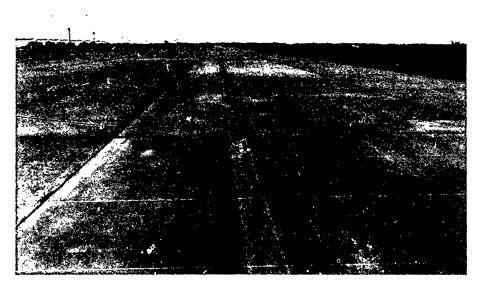


Photo 19. Center slabs replaced on T3A

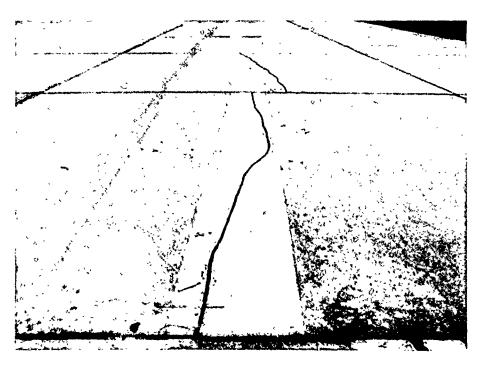


Photo 20. Patched and sealed longitudinal crack on T3A $\,$

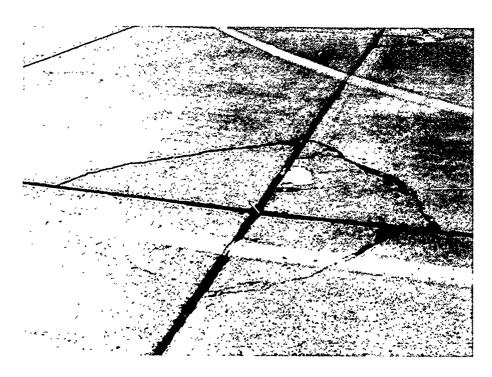


Photo 21. Corner breaks on A31B



Photo 22. Longitudinal/transverse crack on A31B



Photo 23. Spalling corner on A31B



Photo 24. Replaced slabs on T22B



Photo 25. Sawed and sealed joint reflective crack with longitudinal/transverse crack parallel, T14B

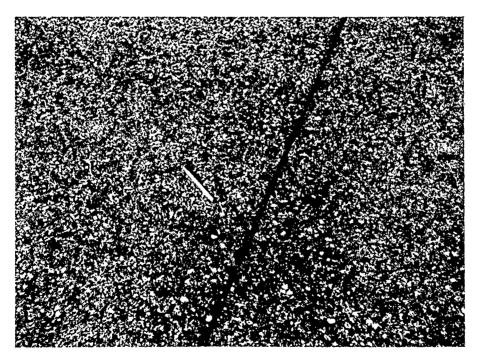


Photo 26. Sawed and sealed reflective joint, T14B

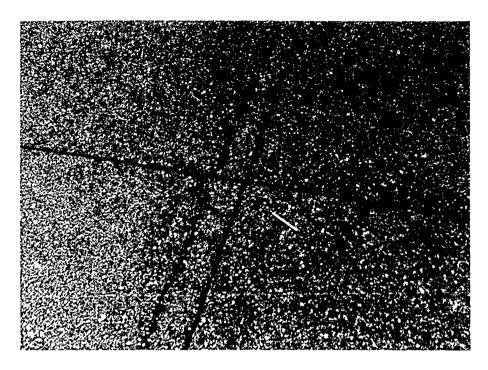


Photo 27. Cracking adjacent to sawed and sealed joint reflective cracking, T14B

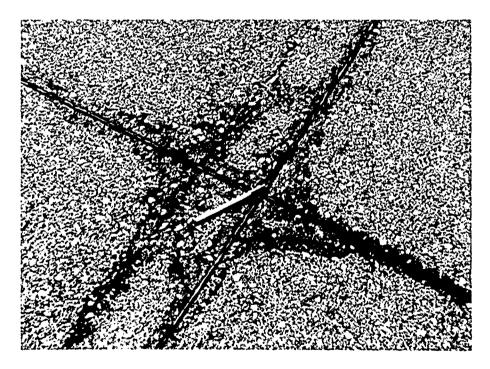


Photo 28. Corner cracking on sawed and sealed joint reflective cracking, T14B

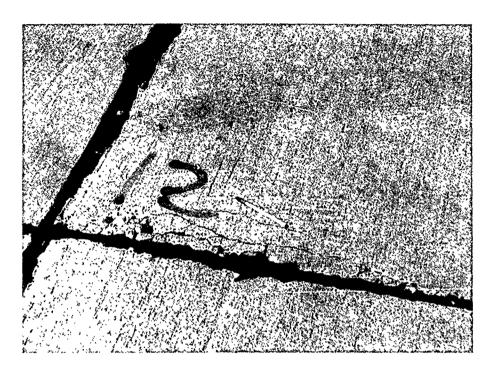


Photo 29. Joint spalling on A10B

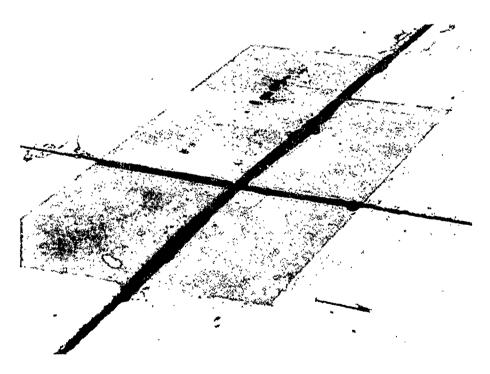


Photo 30. Patching on T4A

APPENDIX A: DATA REPORTS AND ANALYSIS PROGRAMS

BRANCH LISTING

CANNON AFB

BRANCH NUMBER	BRANCH NAME	NUMBER OF SECTIONS
R01A	RUNWAY 04-22	3
	RUNWAY 04-22	3
R03A	RUNWAY 04-22	3
R04A	RUNWAY 13-31	3
R05A	RUNWAY 13-31	3 3
R06C	RUNWAY 13-31	3
R07C	RUNWAY 13-31	3
R08C	RUNWAY 13-31	3
R09C	RUNWAY 13-31	3
R10A	RUNWAY 13-31	3
R11A	RUNWAY 13-31	3
001C	RUNWAY 04-22 OVERRUN	1
002C	RUNWAY 04-22 O'RUN BLAST	1
003C	RUNWAY 04-22 O'RUN BLAST	
004C	RUNWAY 04-22 OVERRUN	1
	RUNWAY 13-31 O'RUN BLAST	
	RUNWAY 13-31 OVERRUN	1
007C	RUNWAY 13-31 O'RUN BLAST	
008C	RUNWAY 13-31 OVERRUN	1
TO3A	TAXIWAYS R AND A	1
T04A	RUNWAY 13-31 OVERRUN TAXIWAYS R AND A TAXIWAY A TAXIWAY B TAXIWAY R	1
T05B	TAXIWAY B	1
1008	IIMLIHIII D	1
T07C	NORTH CAL PAD ACCESS	1
T08A	TAXIWAY E	1
T09A	TAXIWAY R - SOUTH END	1
T10A	TAXIWAY D	1
T11A	TAXIWAY D	1
T12B	TAXIWAY C	1
T13B	TAXIWAY C	2
T14B	TAXIWAY C	1,
T15C	TAXIWAY C - FILLET	1
T16C	TAXIWAY C - PAD ACCESS	1
T17B	TAXIWAY C	1
T18B	TAXIWAY C	1.
T19A	TAXIWAY R AT SOUTH CAL	1
T20A	TAXIWAY R	1
T22B	SOUTH RAMP TAXIWAY	1
AO1B	SOUTH OPERATIONAL APRON	1
AO2B	MAIN OPERATIONAL APRON	1
AO3B	MAIN OPER APRON-WEST EDGE	1
A04B	NORTH OPERATIONAL APRON	1
AO5B	MAINTENANCE APRON	1

A06B	SOUTH EDGE-HANGAR ACCESS	1
A07B	HANGAR ACCESS (BLDG 119)	1
A08B	HANGAR ACCESS APRONS	5
A09B	HANGAR ACCESS (BLDG 184)	2
A10B	MAINTENANCE HANGAR ACCESS	1
AllB	WARM-UP PAD - TAXIWAY D	1
A12B	ARM-DISARM PAD - T/W D	1
A13B	ARM-DISARM ADDITION-T/W D	1
A14B	ARM-DISARM ADDITION-T/W A	1
A15B	ARM-DISARM PAD - T/W A	1
A16B	WARM-UP PAD - TAXIWAY A	1
A18B	WARM-UP PAD - TAXIWAY C	1
A26B	NORTH CALIBRATION PAD	1
A27B	WASHRACK AND ACCESS	1
A28B	POWER CHECK PAD	1
A29B	POWER CHECK PAD	1
A30B	POWER CHECK PAD	1
A31B	SOUTH RAMP	1
ROAD	MAIN OPER APRON ROADWAY	1

INV - INVENTORY REPORT

CANNON AFB

INVENTORY SECTION CATEGORY TYPE

		SURF TYPE	BRANCH USE	PAVEMENT RANK	AREA (SY)
R01A	RUNWAY 04-22 SECTION 1 FROM- CENTER 50 FT-10+00 TO- 20+00	PCC	RUNWAY	PRIMARY	5555
	SECTION 2 FROM- NW 50 FT - 10+00 TO- 20+00	PCC	RUNWAY	PRIMARY	5555
	SECTION 3 FROM- SE 50 FT - 10+00 TO- 20+00	PCC	RUNWAY	PRIMARY	5555
	20.00		TO	TAL BRANCH AREA	16665
RO2C	RUNWAY 04-22 SECTION 1 FROM- CENTER 80 FT-20+00 TO- 100+00	PCC	RUNWAY	PRIMARY	71111
	SECTION 2 FROM- NW 35 FT - 20+00 TO- 100+00	PCC	RUNWAY	PRIMARY	31111
	SECTION 3 FROM- SE 35 FT - 20+00 TO- 100+00	PCC	RUNWAY	PRIMARY	31111
			TOT	CAL BRANCH AREA	133333
R03A	RUNWAY 04-22 SECTION 1 FROM- CENTER 50 FT-100+00 TO- 110+00	PCC	RUNWAY	PRIMARY	5555
	SECTION 2 FROM- NW 50 FT - 100+00 TO- 110+00	PCC	RUNWAY	PRIMARY	5555

	SECTION 3 FROM- SE 50 FT - 100+00 TO- 110+00	PCC	RUNWAY	PRIMARY	5555
				TOTAL BRANCH AREA	16665
R04A	RUNWAY 13-31 SECTION 1 FROM- CENTER 50 FT-10+00 TO- 13+00	PCC	RUNWAY	SECONDARY	1666
	SECTION 2 FROM- NE 50 FT - 10+00 TO- 13+00	PCC	RUNWAY	SECONDARY	1666
	SECTION 3 FROM- SW 50 FT - 10+00 TO- 13+00	PCC	RUNWAY	SECONDARY	1666
			•	TOTAL BRANCH AREA	4998
R05A	RUNWAY 13-31 SECTION 1 FROM- CENTER 50 FT-13+00 TO- 20+00	AC	RUNWAY	PRIMARY	3888
	SECTION 2 FROM- NE 50 FT - 13+00 TO- 20+00	AC	RUNWAY	SECONDARY	3888
	SECTION 3 FROM- SW 50 FT - 13+00 TO- 20+00	AC	RUNWAY	SECONDARY	3888
			r	TOTAL BRANCH AREA	11664
R06C	RUNWAY 13-31 SECTION 1 FROM- CENTER-20+00T028+35 TO- PLUS 29+25 TO 38+50		RUNWAY	SECONDARY	9777
	SECTION 2 FROM- NE 50'-20+00T028+35 TO- PLUS 29+25 TO 38+50		RUNWAY	SECONDARY	9777
	SECTION 3 FROM- SW 50'-20+00T028+35 TO- PLUS 29+25 TO 38+50		RUNWAY	SECONDARY	9777
			T	OTAL BRANCH AREA	29331

				•	
R07C	RUNWAY 13-31 SECTION 1 FROM- CENTER 50 FT-28+35 TO- 29+25	AC 5	RUNWAY	SECONDARY	500
	SECTION 2 FROM- NE 50 FT - 28+35 TO- 29+25	AC	RUNWAY	SECONDARY	500
	SECTION 3 FROM- SW 50 FT - 28+35 TO- 29+25	AC	RUNWAY	SECONDARY	500
			TOTAL	BRANCH AREA	1500
	RUNWAY 13-31 SECTION 1 FROM- CENTER 50 FT-40+00 TO- 46+00	AC	RUNWAY	SECONDARY	3333
,	SECTION 2 FROM- NE 50 FT - 40+00 TO- 46+00	AC	RUNWAY	SECONDARY	3333
	SECTION 3 FROM- SW 50 FT - 40+00 TO- 46+00	AC	RUNWAY	SECONDARY	3333
			TOTAL	BRANCH AREA	9999
	RUNWAY 13-31 SECTION 1 FROM- CENTER 50 FT-46+00 TO- 82+00	AC	RUNWAY	SECONDARY	20000
	SECTION 2 FROM- NE 50 FT - 46+00 TO- 82+00	AC	RUNWAY	SECONDARY	20000
	SECTION 3 FROM- SW 50 FT - 46+00 TO- 82+00	AC	RUNWAY	SECONDARY	20000
			TOTAL	BRANCH AREA	60000
	RUNWAY 13-31 SECTION 1 FROM- CENTER 50 FT-82+00 TO- 89+00	AC	RUNWAY	SECONDARY	3888

	SECTION 2 FROM- NE 50 FT - 82+00 TO- 89+00	AC	RUNWAY	SECONDARY	3888
	SECTION 3 FROM- SW 50 FT - 82+00 TO- 89+00	AC	RUNWAY	SECONDARY	3888
	10- 89+00		TOTAL	BRANCH AREA	11664
R11A	RUNWAY 13-31 SECTION 1 FROM- CENTER 50 FT-89+00 TO- 92+00	PCC	RUNWAY	SECONDARY	1666
	SECTION 2 FROM- NE 50 FT - 89+00 TO- 92+00	PCC	RUNWAY	SECONDARY	1666
	SECTION 3 FROM- SW 50 FT - 89+00 TO- 92+00	PCC	RUNWAY	SECONDARY	1666
	10- 92+00		TOTAL	BRANCH AREA	4998
001C	RUNWAY 04-22 OVERRUN SECTION 1 FROM- 111+50 TO- 120+00	ST	RUNWAY	SECONDARY	14166
	10- 120100		TOTAL	BRANCH AREA	14166
002C	RUNWAY 04-22 O'RUN BLAST SECTION 1 FROM- 110+00 TO- 111+50	AC	RUNWAY	SECONDARY	2500
	10- 111-50		TOTAL	BRANCH AREA	2500
003C	RUNWAY 04-22 O'RUN BLAST SECTION 1 FROM- 8+50	AC	RUNWAY	SECONDARY	2500
	TO- 10+00		TOTAL	BRANCH AREA	2500
004C	RUNWAY 04-22 OVERRUN SECTION 1 FROM- 0+00	ST	RUNWAY	SECONDARY	14166
	TO- 8+50		TOTAL	BRANCH AREA	14166

005C	RUNWAY 13-31 O'RUN BLAST SECTION 1 FROM- 8+50 TO- 10+00	AC	RUNWAY	SECONDARY	2500
			TOTAL	BRANCH AREA	2500
	RUNWAY 13-31 OVERRUN SECTION 1 FROM- 0+00 TO- 8+50	ST	RUNWAY	SECONDARY	14166
			TOTAL	BRANCH AREA	14166
007C	RUNWAY 13-31 O'RUN BLAST SECTION 1 FROM- 92+00 TO- 93+50		RUNWAY	SECONDARY	2500
	10 93130		TOTAL	BRANCH AREA	2500
0080	OOSC RUNWAY 13-31 OVERRUN SECTION 1 FROM- 93+50	ST	RUNWAY	SECONDARY	14166
	TO- 102+00		TOTAL	BRANCH AREA	14166
тоза	TO3A TAXIWAYS R AND A SECTION 1 FROM- FEATURE TO4A	PCC	TAXIWAY	PRIMARY	23333
	TO- FEATURE T20A		TOTAL	BRANCH AREA	23333
T04A	TAXIWAY A				
	SECTION 1 FROM- RUNWAY 04-22	PCC	TAXIWAY	PRIMARY	9483
	TO- FEATURE TO3A		TOTAL	BRANCH AREA	9483
т05В	TAXIWAY B SECTION 1 FROM- TO-	PCC	TAXIWAY	SECONDARY	2961
	10-		TOTAL	BRANCH AREA	2961

	TAXIWAY B SECTION 1 FROM- TO-	AC	TAXIWAY	SECONDARY	7166
			TOTAL	BRANCH AREA	7166
	NORTH CAL PAD ACCESS SECTION 1 FROM- TO-	AC	TAXIWAY	SECONDARY	16041
			TOTAL	BRANCH AREA	16041
	TAXIWAY E SECTION 1 FROM-	PCC	TAXIWAY	PRIMARY	14440
	TO-		ΤΟΤΑΙ.	BRANCH AREA	14440
			101112	DIGHTOH THEBET	24440
SEC FRO	TAXIWAY R - SOUTH END SECTION 1 FROM- 260 FT NE OF 13-31 TO- FEATURE T19A	PCC	TAXIWAY	PRIMARY	5333
	10- FERIORE 119A		TOTAL.	BRANCH AREA	5333
			101112		5555
T10A	TAXIWAY D				
	SECTION 1		TAXIWAY	PRIMARY	3500
	FROM- 160 FT SW OF 13-31 TO- 260 FT NE OF 13-31				
	10- 200 F1 NE OF 15-51		TOTAL	BRANCH AREA	3500
T11A	TAXIWAY D				
	SECTION 1	PCC	TAXIWAY	PRIMARY	20141
	FROM- EDGE OF RW 04-22 TO- 160 FT SW OF 13-31				
	10- TOO LT 2M OL 13-31		TOTAL	BRANCH AREA	20141
			202118		
T12B	TAXIWAY C				
	SECTION 1	APC	TAXIWAY	SECONDARY	9439
	FROM- FEATURE T20A TO- FEATURE T13B				
	10- FEATURE 113B		ΤΩΤΑΙ	BRANCH AREA	9439
			101111		,

T13B	TAXIWAY C SECTION 1 FROM- NW EDGE-RW 04-22 TO- 1+75	PCC	TAXIWAY	SECONDARY	2531
	SECTION 2 FROM- SE EDGE-RW 04-22	PCC	TAXIWAY	SECONDARY	1727
	TO- 1+75		TOTAL	BRANCH AREA	4258
T14B	FROM- FEATURE T13B	APC	TAXIWAY	SECONDARY	28782
	TO- FEATURE T17B		TOTAL	BRANCH AREA	28782
	15C TAXIWAY C - FILLET SECTION 1 A		TAXIWAY	SECONDARY	1313
	TO-		TOTAL	BRANCH AREA	1313
T16C	T16C TAXIWAY C - PAD ACCESS SECTION 1 FROM-	AC	TAXIWAY	SECONDARY	2333
	TO-		TOTAL	BRANCH AREA	2333
T17B	TAXIWAY C SECTION 1 FROM- FEATURE T18B	AC	TAXIWAY	SECONDARY	17833
	TO- FEATURE T14B		TOTAL	BRANCH AREA	17833
T18B	TAXIWAY C SECTION 1 FROM- SE END - RW 13-31	AC	TAXIWAY	SECONDARY	75
	TO- FEATURE T17B		TOTAL	BRANCH AREA	075
~					
T19A	TAXIWAY R AT SOUTH CAL SECTION 1 FROM- FEATURE TO9A	PCC	TAXIWAY	PRIMARY	3166
	TO- FEATURE T10A		TOTAL	BRANCH AREA	3166

T20A TAXIWAY R SECTION 1 FROM- FEATURE T19A		PCC	TAXIWA	TAXIWAY		PRIMARY	
	TO- FEATURE TO3A			TOTAL	BRANCH	AREA	28000
Т22В	SOUTH RAMP TAXIVAY SECTION 1 FROM-	PCC	TAXIWA	ΛY	SECON	IDARY	10291
	то-			TOTAL	BRANCH	AREA	10291
	SOUTH OPERATIONAL APRON SECTION 1 FROM-	PGG	APRON		SECON	IDARY	20416
	то-			TOTAL	BRANCH	AREA	20416
	MAIN OPERATIONAL APRON SECTION 1 FROM- TO-	PCC	APRON		PRIMA	ARY	180000
				TOTAL	BRANCH	AREA	180000
A03B	MAIN OPER APRON-WEST EDGI SECTION 1 FROM-		APRON		SECON	IDARY	17638
	TO-			TOTAL	BRANCH	AREA	17638
A04B	NORTH OPERATIONAL APRON SECTION 1 FROM-	PCC	APRON		SECON	IDARY	71139
	TO-			TOTAL	BRANCH	AREA	71139
A05B	MAINTENANCE APRON SECTION 1 FROM- TO-	PCC	APRON		SECON	IDARY	9111
	10-			TOTAL	BRANCH	AREA	9111

	SOUTH EDGE-HANGAR ACCESS SECTION 1 FROM- TO-	PCC	APRON	SECONDARY	1500
		•		TOTAL BRANCH AREA	1500
A07B	HANGAR ACCESS (BLDG 119) SECTION 1 FROM- TO-	PCC	APRON	SECONDARY TOTAL BRANCH AREA	4283 4283
				TOTAL DIGITOR TRADE	4203
A08B	HANGAR ACCESS APRONS SECTION 1 FROM- BUILDING 108 TO-	PCC	APRON	SECONDARY	1478
	SECTION 2 FROM- BUILDING 121 TO-	PCC	APRON	SECONDARY	1523
	SECTION 3 FROM- BUILDING 129 TO-	PCC	APRON	SECONDARY	1499
	SECTION 4 FROM- BUILDING 162 TO-	PCC	APRON	SECONDARY	1444
	SECTION 5 FROM- BUILDING 170	PCC	APRON	SECONDARY	1559
	TO-			TOTAL BRANCH AREA	7503
109B	HANGAR ACCESS (BLDG 184) SECTION 1 FROM- TO-	PCC	APRON	SECONDARY	2311
	SECTION 2 FROM- TO-	PCC	APRON	SECONDARY	2311
				TOTAL BRANCH AREA	4622

A10B	MAINTENANCE HANGAR ACCESS SECTION 1 FROM- BUILDINGS 195 196 TO- AND 197		APRON	SECONDARY			11183
	10 11.0 12.1			TOTAL	BRANCH	AREA	11183
A11B	WARM-UP PAD - TAXIWAY D SECTION 1 FROM-TO-	PCC	APRON		SECON	DARY	4728
	10-			TOTAL	BRANCH	AREA	4728
A12B	ARM-DISARM PAD - T/W D SECTION 1 FROM-	PCC	APRON		SECON	DARY	5867
	TO-			TOTAL	BRANCH	AREA	5867
A13B	ARM-DISARM ADDITION-T/W SECTION 1 FROM- TO-	D PCC	APRON		SECON	JDARY	1777
	10-			TOTAL	BRANCH	AREA	1777
A14B	ARM-DISARM ADDITION-T/W A SECTION 1 FROM-	A PCC	APRON		SECON	DARY	1777
	TO-			TOTAL	BRANCH	AREA	1777
A15B	SECTION 1 FROM-	PCC	APRON		SECON	IDARY	5956
	TO-			TOTAL	BRANCH	AREA	5956
A16B	WARM-UP PAD - TAXIWAY A SECTION 1 FROM-	PCC	APRON		SECON	IDARY	3831
	TO-			TOTAL	BRANCH	AREA	3831

A18B	WARM-UP PAD - TAXIWAY C SECTION 1 FROM- TO-	PCC	APRON		SECON	IDARY	4728
		•		TOTAL	BRANCH	AREA	4728
A26B	NORTH CALIBRATION PAD SECTION 1 FROM- TO-	PCC	APRON		SECON	DARY	6600
				TOTAL	BRANCH	AREA	6600
A27B	WASHRACK AND ACCESS SECTION 1 FROM- TO-	PCC	APRON		SECON	DARY	3719
				TOTAL	BRANCH	AREA	3719
A28B	POWER CHECK PAD SECTION 1 FROM-	PCC	APRON		SECON	DARY	1833
	ТО-			TOTAT	DDANCH	ADEA	1022
				IOIAL	BRANCH	AKEA	1833
A29B	POWER CHECK PAD SECTION 1 FROM- TO-	PCC	APRON		SECON	DARY	1556
				TOTAL	BRANCH	AREA	1556
V30b	DOLLED CHECK DAD						
auca	POWER CHECK PAD SECTION 1 FROM- TO-	PCC	APRON		SECON	DARY	1844
	10-			тотат.	BRANCH	AREA	1844
					2141011		40-7-7
1035	COUNTY DAVE						
A31B	SOUTH RAMP SECTION 1 FROM-	PCC	APRON		SECON	DARY	24700
	TO-			TOTAL	BRANCH	AREA	24700

ROAD MAIN OPER APRON ROADWAY
SECTION 1 PCC APRON SECONDARY 10583
FROMTOTOTAL BRANCH AREA 10583

TOTAL AREA OF SELECTED SECTION CATEGORY PAVEMENTS 982,263

PCI REPORT

CANNON AFB

BRANCH NUMBER/NAME	SECTION NUMBER.		F. SECTION AREA/SY	INSPECTION DATE	PCI
•					
RO1A *RUNWAY 04-22	1	PRIMARY PCC	5 5555	06/02/89	23
BRANCH USE- RUNV	YAY [FROM] - CENT	ER 50 FT-10+00	[TO]-20-	+00	
RO1A *RUNWAY 04-22	2	PRIMARY PCC	5555	06/02/89	63
BRANCH USE- RUNW	YAY [FROM] - NW 50	FT - 10+00	[TO]-20+	-00	
RO1A *RUNWAY 04-22	3	PRIMARY PCC	5555	06/02/89	58
BRANCH USE- RUNW	YAY [FROM] - SE 50	FT - 10+00	[TO]-20+	-00	
RO2C *RUNWAY 04-22	1	PRIMARY PCC	71111	06/02/89	49
BRANCH USE- RUNW	AY [FROM] - CENTE	ER 80 FT-20+00	[TO]-100	+00	
RO2C *RUNWAY 04-22	2	PRIMARY PCC	31111	06/02/89	89
BRANCH USE- RUNW	YAY [FROM] - NW 35	5 FT - 20+00	[TO]-100	+00	
RO2C *RUNWAY 04-22	3	PRIMARY PCC	31111	06/02/89	87
BRANCH USE- RUNW	YAY [FROM] - SE 35	5 FT - 20+00	[TO]-100	+00	
RO3A *RUNWAY 04-22	1	PRIMARY PCC	5555	06/02/89	73
BRANCH USE- RUNW	AY [FROM] - CENTE	ER 50 FT-100+00	O [TO]-110	0+00	
RO3A *RUNWAY 04-22	2	PRIMARY PCC	5555	06/02/89	75
BRANCH USE- RUNW	AY [FROM] - NW 50	FT - 100+00	[TO]-110)+00	
RO3A *RUNWAY 04-22	3	PRIMARY PCC	5555	06/02/89	88
BRANCH USE- RUNWA	Y [FROM] - SE 50	FT - 100+00	[TO]-110	+00	
RO4A *RUNWAY 13-31	1	SECONDARY PCC	1666	06/02/89	19
BRANCH USE- RUNW	AY [FROM] - CENTE	ER 50 FT-10+00	[TO]-13+	-00	
RO4A *RUNWAY 13-31	2	SECONDARY PCC	1666	06/02/89	50
BRANCH USE- RUNW	AY [FROM] - NE 50	FT - 10+00	[TO]-13+	00	

RO4A *RUNWAY 13-31	3 SECONDARY PCC	1666 06/02/89	56
BRANCH USE- RUNWAY	[FROM] - SW 50 FT - 10+00	[TO]-13+00	
RO5A *RUNWAY 13-31	1 PRIMARY AC	3888 06/02/89	76
BRANCH USE- RUNWAY	[FROM] - GENTER 50 FT-13+00	[TO]-20+00	
RO5A *RUNWAY 13-31	2 SECONDARY AC	3888 06/02/89	88
BRANCH USE- RUNWAY	[FROM] - NE 50 FT - 13+00	[TO]-20+00	
RO5A *RUNWAY 13-31	3 SECONDARY AC	3888 06/02/89	85
BRANCH USE- RUNWAY	[FROM] - SW 50 FT - 13+00	[TO]-20+00	
RO6C *RUNWAY 13-31	i SECONDARY AC	9777 06/02/89	89
BRANCH USE- RUNWAY	[FROM] - CENTER-20+00T023+35	[TO]-PLUS 29+25 TO	38+50
R06C *RUNWAY 13-31	2 SECONDARY AC	9777 06/02/89	83
BRANCH USE- RUNWAY	[FROM] - NE 50'-20+00T028+35	[TO]-PLUS 29+25 TO	38+50
RO6C *RUNWAY 13-31	3 SECONDARY AC	9777 06/02/89	84
BRANCH USE- RUNWAY	[FROM] - SW 50'-20+00T028+35	[TO]-PLUS 29+25 TO	38+50
R07C *RUNWAY 13-31	1 SECONDARY AC	500 06/02/89	88
BRANCH USE- RUNWAY	[FROM] - CENTER 50 FT-28+35	[TO]-29+25	
R07C *RUNWAY 13-31	2 SECONDARY AC	500 06/02/89	84
BRANCH USE- RUNWAY	[FROM] - NE 50 FT - 28+35	[TO]-29+25	
R07C *RUNWAY 13-31	3 SECONDARY AC	500 06/02/89	79
BRANCH USE- RUNWAY	[FROM] - SW 50 FT - 28+35	[TO]-29+25	
ROSC *RUNWAY 13-31	1 SECONDARY AC	3333 06/02/89	96
BRANCH USE- RUNWAY	[FROM] - CENTER 50 FT-40+00	[TO]-46+00	
ROSC *RUNWAY 13-31	2 SECONDARY AC	3333 06/02/89	91
BRANCH USE- RUNWAY	[FROM] - NE 50 FT - 40+00	[TO]-46+00	
ROSC *RUNWAY 13-31	3 SECONDARY AC	3333 06/02/89	92
BRANCH USE- RUNWAY	[FROM] - SW 50 FT - 40+00	[TO]-46+00	

R09C *RUNWAY	13-31	1	SECONDARY AC	20000	06/02/89	91
BRANCH USE-	RUNWAY	[FROM] - CENTE	R 50 FT-46+00	[TO]-82	+00	
R09C *RUNWAY	13-31	2	SECONDARY AC	20000	06/02/89	90
BRANCH USE-	RUNWAY	[FROM] - NE 50	FT - 46+00	[TO]-82	2+00	
R09C *RUNWAY	13-31	3	SECONDARY AC	20000	06/02/89	93
BRANCH USE-	RUNWAY	[FROM] - SW 50	FT - 46+00	[TO]-82	2+00	
R10A *RUNWAY	13-31	1	SECONDARY AC	3888	06/02/89	81
BRANCH USE-	RUNWAY	[FROM] - CENTE	R 50 FT-82+00	[TO]-89	+00	
R10A *RUNWAY	13-31	2	SECONDARY AC	3888	06/02/89	83
BRANCH USE-	RUNWAY	[FROM] - NE 50	FT - 82+00	[TO]-89	9+00	
R10A *RUNWAY	13-31	3	SECONDARY AC	3888	06/02/89	86
BRANCH USE-	RUNWAY	[FROM] - SW 50	FT - 82+00	[TO]-89	9+00	
R11A *RUNWAY	13-31	1	SECONDARY PCC	1666	06/02/89	44
BRANCH USE-	RUNWAY	[FROM] - CENTE	R 50 FT-89+00	[TO]-92	2+00	
R11A *RUNWAY	13-31	2	SECONDARY PCC	1666	06/02/89	49
BRANCH USE-	RUNWAY	[FROM] - NE 50	FT - 89+00	[TO]-92	2+00	
R11A *RUNWAY	13-31	3	SECONDARY PCC	1666	06/02/89	76
BRANCH USE-	RUNWAY	[FROM] - SW 50	FT - 89+00	[TO]-92	2+00	
001C *RUNWAY	04-22 OVERR	un 1	SECONDARY ST	14166	06/02/89	78
BRANCH USE-	RUNWAY	[FROM] - 111+5	0	[TO]-12	20+00	
002C *RUNWAY	04-22 O'RUN	BLAST 1	SECONDARY AC	2500	06/02/89	84
BRANCH USE-	RUNWAY	[FROM]- 110+0	0	[TO]-11	L1+50	
003C *RUNWAY	04-22 O'RUN	BLAST 1	SECONDARY AC	2500	06/02/89	80
BRANCH USE-	RUNWAY	[FROM] - 8+50		[TO]-10	0+00	
004C *RUNWAY	04-22 OVERR	UN 1	SECONDARY ST	14166	06/02/89	86
BRANCH USE-	RUNWAY	[FROM]- 0+00		[TO]-8+	⊦ 50	

OO5C *RUNWAY	13-31 O'RUN	BLAST 1	SECONDARY	AC	2500	06/02/89	64
BRANCH USE-	RUNWAY	[FROM]- 8	+50		[TO]-10+	00	
006C *RUNWAY	13-31 OVER	RUN 1	SECONDARY	ST 1	.4166	06/02/89	66
BRANCH USE-	RUNWAY	[FROM]- 0	+00		[TO]-8+5	0	
007C *RUNWAY	13-31 O'RUN	BLAST 1	SECONDARY	AC	2500	06/02/89	58
BRANCH USE-	RUNWAY	[FROM]- 9	2+00		[TO]-93	+50	
008C *RUNWAY	13-31 OVERF	RUN 1	SECONDARY	ST 1	4166	06/02/89	92
BRANCH USE-	RUNWAY	[FROM]- 9	3+50		[TO]-10	2+00	
TO3A *TAXIWAY	S R AND A	1	PRIMARY	PCC 2	23333	06/02/89	59
BRANCH USE-	TAXIWAY	[FROM]- F	FEATURE TO4A		[TO]-FE	ATURE T20A	
TO4A *TAXIWAY	A	1	PRIMARY	PCC	9483	06/02/89	80
BRANCH USE-	TAXIWAY	[FROM]-		ĺ	TO]-		
TO5B *TAXIWA	Y B	1	SECONDARY	PCC	2961	06/02/89	65
BRANCH USE-	TAXIWAY	[FROM] - F	RUNWAY 04-22		[TO]-FE	ATURE TO3A	<u>.</u>
TO6B *TAXIWAY		[FROM] - F				06/02/89	
	В	1		AC			
TO6B *TAXIWAY BRANCH USE-	B TAXIWAY	1 [FROM]-		AC [7166 TO]-		59
TO6B *TAXIWAY BRANCH USE-	B TAXIWAY AL PAD ACCE	[FROM]-	SECONDARY	AC I	7166 TO]-	06/02/89	59
TO6B *TAXIWAY BRANCH USE- TO7C *NORTH CA	B TAXIWAY AL PAD ACCE TAXIWAY	[FROM]-	SECONDARY	AC [7166 TO]- .6041 TO]-	06/02/89	59
TO6B *TAXIWAY BRANCH USE- TO7C *NORTH CA BRANCH USE-	B TAXIWAY AL PAD ACCE TAXIWAY E	1 [FROM] - ESS 1 [FROM] -	SECONDARY	AC 1 PCC 1	7166 TO]- .6041 TO]-	06/02/89	45
BRANCH USE- TO7C *NORTH CA BRANCH USE- TO8A *TAXIWAY	B TAXIWAY AL PAD ACCE TAXIWAY E TAXIWAY	[FROM] - [FROM] - [FROM] -	SECONDARY	AC 1 PCC 1	7166 TO]- .6041 TO]- .4440	06/02/89	45
TO6B *TAXIWAY BRANCH USE- TO7C *NORTH CA BRANCH USE- TO8A *TAXIWAY BRANCH USE-	B TAXIWAY AL PAD ACCE TAXIWAY E TAXIWAY R - SOUTH	1 [FROM] - ESS 1 [FROM] - 1 [FROM] - END 1	SECONDARY SECONDARY PRIMARY	AC [[PCC]	7166 TO]- .6041 TO]- .4440 TO]-	06/02/89 06/02/89 06/02/89	59 45 96
TO6B *TAXIWAY BRANCH USE- TO7C *NORTH CA BRANCH USE- TO8A *TAXIWAY BRANCH USE- TO9A *TAXIWAY	B TAXIWAY AL PAD ACCE TAXIWAY E TAXIWAY R - SOUTH TAXIWAY	1 [FROM] - ESS 1 [FROM] - 1 [FROM] - END 1	SECONDARY SECONDARY PRIMARY	AC [[PCC]	7166 TO]- .6041 TO]- .4440 TO]-	06/02/89 06/02/89 06/02/89 06/02/89 ATURE T19A	59 45 96
TO6B *TAXIWAY BRANCH USE- TO7C *NORTH CA BRANCH USE- TO8A *TAXIWAY BRANCH USE- TO9A *TAXIWAY BRANCH USE-	B TAXIWAY AL PAD ACCE TAXIWAY E TAXIWAY R - SOUTH TAXIWAY D	1 [FROM] - ESS 1 [FROM] - 1 [FROM] - END 1 [FROM] - 2	SECONDARY SECONDARY PRIMARY PRIMARY 60 FT NE OF 13	AC [AC] [PCC] PCC PCC	7166 T0]6041 T0]4440 T0]- 5333 [T0]-FEA	06/02/89 06/02/89 06/02/89 06/02/89 ATURE T19A	59 45 96 82
BRANCH USE- TO7C *NORTH CA BRANCH USE- TO8A *TAXIWAY BRANCH USE- TO9A *TAXIWAY BRANCH USE- TO9A *TAXIWAY	B TAXIWAY AL PAD ACCE TAXIWAY E TAXIWAY R - SOUTH TAXIWAY D TAXIWAY	1 [FROM] - ESS 1 [FROM] - 1 [FROM] - END 1 [FROM] - 2	SECONDARY SECONDARY PRIMARY PRIMARY 60 FT NE OF 13 PRIMARY	AC [[AC] [PCC] [PCC] PCC PC	7166 TO]- 6041 TO]- 4440 TO]- 5333 [TO]-FEA 3500 TO]-260	06/02/89 06/02/89 06/02/89 ATURE T19A 06/02/89	59 45 96 82

T12B *TAXIWAY	С	·	1	SECONDARY	APC	9439	06/02/89	59
BRANCH USE-	TAXIWAY	[FROM]	- FEAT	URE T20A		[TO]-F	EATURE T13B	
T13B *TAXIWAY	С		1	SECONDARY	PCC	2531	06/02/89	85
BRANCH USE-	TAXIWAY	[FROM]	- NW E	DGE-RW 04-2	.2	[TO]-1	+75	
T14B *TAXIWAY	С		1	SECONDARY	APC	28782	06/02/89	59
BRANCH USE-	TAXIWAY	[FROM]	- FEAT	URE T13B		[TO]-F	EATURE T17B	
T15C *TAXIWAY	C - FILLET	r ·	1	SECONDARY	AC	1313	06/02/89	58
BRANCH USE-	TAXIWAY	[FROM]-				[TO]-		
T16C *TAXIWAY	C - PAD AC	CCESS	1	SECONDARY	AC	2333	06/02/89	77
BRANCH USE-	TAXIWAY	[FROM]-				[TO]-		
T17B *TAXIWAY	С		1	SECONDARY	AC	17833	06/02/89	69
BRANCH USE-	TAXIWAY	[FROM]	- FEAT	URE T18B		[TO]-F	FEATURE T14B	
T18B *TAXIWAY	С		1	SECONDARY	AC	075	06/02/89	77
BRANCH USE-	TAXIWAY	[FROM]	- SE E	ND - RW 13-	-31	[TO]-F	EATURE T17B	
T19A *TAXIWAY	R AT SOUTH	CAL	1	PRIMARY	PCC	3166	06/02/89	62
BRANCH USE-	TAXIWAY	[FROM]	- FEAT	URE TO9A		[TO]-F	EATURE T10A	
BRANCH USE- T20A *TAXIWAY				URE TO9A PRIMARY			06/02/89	64
	R		1	PRIMARY	PCC	28000	06/02/89	64
T20A *TAXIWAY	R TAXIWAY	[FROM]	1	PRIMARY	PCC	28000	06/02/89	58
T20A *TAXIWAY BRANCH USE-	R TAXIWAY	[FROM]	1 - FEAT	PRIMARY URE T19A	PCC	28000 [TO]-F	06/02/89 FEATURE T03A	
T20A *TAXIWAY BRANCH USE- T22B *SOUTH RA	R TAXIWAY MP TAXIWAY TAXIWAY	[FROM]	1 - FEAT	PRIMARY URE T19A	PCC	28000 [TO]-F	06/02/89 FEATURE T03A	
T20A *TAXIWAY BRANCH USE- T22B *SOUTH RA BRANCH USE-	R TAXIWAY AMP TAXIWAY TAXIWAY PERATIONAL	[FROM]	1 - FEAT	PRIMARY URE T19A SECONDARY	PCC	28000 [TO]-F 10291 [TO]-	06/02/89 PEATURE T03A 06/02/89	58
T20A *TAXIWAY BRANCH USE- T22B *SOUTH RA BRANCH USE- A01B *SOUTH OF	R TAXIWAY MP TAXIWAY TAXIWAY PERATIONAL APRON	[FROM] - [FROM] - APRON [FROM] -	1 - FEAT	PRIMARY URE T19A SECONDARY	PCC	28000 [TO]-F 10291 [TO]- 20416	06/02/89 PEATURE T03A 06/02/89	58
T20A *TAXIWAY BRANCH USE- T22B *SOUTH RA BRANCH USE- A01B *SOUTH OF BRANCH USE-	R TAXIWAY TAXIWAY TAXIWAY PERATIONAL APRON ERATIONAL A	[FROM] - [FROM] - APRON [FROM] -	1 - FEAT 1 - 1	PRIMARY URE T19A SECONDARY SECONDARY	PCC PCC	28000 [TO]-F 10291 [TO]- 20416 [TO]-	06/02/89 FEATURE T03A 06/02/89 06/02/89	58 83
T20A *TAXIWAY BRANCH USE- T22B *SOUTH RA BRANCH USE- A01B *SOUTH OF BRANCH USE- A02B *MAIN OPE	R TAXIWAY TAXIWAY TAXIWAY PERATIONAL APRON ERATIONAL A	[FROM] - [FROM] - APRON [FROM] - PRON [FROM] -	1 - FEAT 1	PRIMARY URE T19A SECONDARY SECONDARY	PCC PCC	28000 [TO]-F 10291 [TO]- 20416 [TO]- 180000	06/02/89 FEATURE T03A 06/02/89 06/02/89	58 83

A04B *NORTH OPER	ATIONAL	APRON	1	SECONDARY	PCC	71139	06/02/89	73
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A05B *MAINTENANC	E APRON		1	SECONDARY	PCC	9111	06/02/89	75
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A06B *SOUTH EDGE	-HANGAR	ACCESS	1	SECONDARY	PCC	1500	06/02/89	83
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A07B *HANGAR ACC	ESS (BLD	G 119)	1	SECONDARY	PCC	4283	06/02/89	90
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A09B *HANGAR ACC	ESS (BLD	G 184)	1	SECONDARY	PCC	2311	06/02/89	82
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A10B *MAINTENANC	E HANGAR	ACCESS	51	SECONDARY	PCC	11183	06/02/89	63
BRANCH USE- AP	RON	[FROM]	- BUILD	INGS 195 1	96	[TO]-ANI	197	
AllB *WARM-UP PA	D - TAXI	WAY D	1	SECONDARY	PCC	4728	06/02/89	77
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A12B *ARM-DISARM	PAD - T	/W D	1	SECONDARY	PCC	5867	06/02/89	56
BRANCH USE- AP	RON	[FROM]	•			[TO]-		
A13B *ARM-DISARM	ADDITIO	N-T/W I	01	SECONDARY	PCC	1777	06/02/89	68
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A14B *ARM-DISARM	ADDITIO	N-T/W A	<u> </u>	SECONDARY	PCC	1777	06/02/89	69
BRANCH USE- AP	RON	[FROM]	•			[TO]-		
A1.5B *ARM-DISARM	PAD - T	/W A	1	SECONDARY	PCC	5956	06/02/89	74
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A16B *WARM-UP PA	D - TAXI	WAY A	1	SECONDARY	PCC	3831	06/02/89	82
BRANCH USE- AP	RON	[FROM]	-			[TO]-		
A18B *WARM-UP PA	D - TAXI	WAY C	1	SECONDARY	PCC	4728	06/02/89	69
BRANCH USE- AP	RON	[FROM]	-			[TO]-		

A26B *NORTH	CALIBRATION	PAD	1	SECONDARY	PCC	6600	06/02/89	42
BRANCH USE	- APRON	[FROM]	-			[TO]-		
A27B *WASHRA	ACK AND ACCE	SS	1 .	SECONDARY	PCC	3719	06/02/89	92
BRANCH USE	- APRON	[FROM]	-			[TO]-		
A28B *POWER	CHECK PAD		1	SECONDARY	PCC	1833	06/02/89	56
BRANCH USE	- APRON	[FROM]	-			[TO]-		
A29B *POWER	CHECK PAD		1	SECONDARY	PCC	1556	06/02/89	68
BRANCH USE-	- APRON	[FROM]	-			[TO]-		
A30B *POWER	CHECK PAD		1	SECONDARY	PCC	1844	06/02/89	61
BRANCH USE-	- APRON	[FROM]	-			[TO]-		
A31B *SOUTH	RAMP	<u> </u>	1	SECONDARY	PCC	24700	06/02/89	31
BRANCH USE-	- APRON	[FROM]	-			[TO]-		
ROAD *MAIN (PER APRON RO	DADWAY	1	SECONDARY	PCC	10583	06/02/89 '	74
BRANCH USE-	- APRON	[FROM]	-			[TO]-		

INSPECTION SCHEDULE REPORT

FY TO	NO. OF SE	CT.	PAVEMENT	RANK
INSP.	TO INSP.	PRIMARY	SECONDARY	TERTIARY
1989	0	0	0	0
1990	2	0	2	0
1991	0	0	0	0
1992	31	6	25	0
1993	0	0	0	0
1994	50	13	37	0

TOTAL NO. OF SECTION: 83
SECT. NOT NEEDING INSPECTION: 0
NO. OF MISSING VALUE: 7

NO. SEC.	FY TO INSP.	!				
0	1989	!!!!!!				
2	1990	! !** !				
0	1991	!!!!!				
31	1992	! !********* !	*****	****		
0	1993	!!!!				
50	1994	! !******** !	******	******	******	ት
		!	!	!	!	!
83		0	13	26	39	52

NO OF SECTIONS

TOTAL NO. OF SECTION: 83
SECT. NOT NEEDING INSPECTION: 0
NO. OF MISSING VALUE: 7

LIST OF CASES IN INSPECTION SCHEDULE REPORT

	FY TO	O INSPE	CT : 19	990	NO. OF SECTIONS : 2	
BRANCH	BRANCH	SECT.	PAVE.	SUT		TO
NUMBER	USE	NO.	RANK		AREA	
T15C	TAXIWAY	1	S	AC	1313	
A09B	APRON	1	S	PCC	2311	
			CT : 19		NO. OF SECTIONS: 31	
BRANCH			PAVE.	SUT	SEC FROM	TO
NUMBER		NO.	RANK		AREA	
R01A	RUNWAY	1	P	PCC	5555 CENTER 50 FT-10+00	
R03A	RUNWAY	1	P	PCC	5555 CENTER 50 FT-100+00	110+00
R03A	RUNWAY	2	P	PCC	5555 NW 50 FT - 100+00	110+00
R04A	RUNWAY	1	S	PCC	1666 CENTER 50 FT-10+00	
R05A	RUNWAY	1	P	AC	3888 CENTER 50 FT-13+00	20+00
R05A	RUNWAY	2	S	AC	3888 NE 50 FT - 13+00	20+00
R05A	RUNWAY	3	S	AC	3888 SW 50 FT - 13+00	20+00
R06C	RUNWAY	1	S	AC	9777 CENTER-20+00T028+35	PLUS 29+25 TO 38+50
R06C	RUNWAY	2	S	AC	9777 NE 50'-20+00T028+35	PLUS 29+25 TO 38+50
R06C	RUNWAY	3	S	AC	9777 SW 50'-20+00T028+35	PLUS 29+25 TO 38+50
R07C	RUNWAY	1	S	AC	500 CENTER 50 FT-28+35	29+25
R07C	RUNWAY	2	S	AC	500 NE 50 FT - 28+35	29+25
R07C	RUNWAY	3	S	AC	500 SW 50 FT - 28+35	29+25
R08C	RUNWAY	2	S	AC	3333 NE 50 FT - 40+00	46+00
R09C	RUNWAY	1	S	AC	20000 CENTER 50 FT-46+00	82+00
R09C	RUNWAY	2	S	AC	20000 NE 50 FT - 46+00	82+00
R10A	RUNWAY	1	S	AC	3888 CENTER 50 FT-82+00	89+00
R10A	RUNWAY	2	S	AC	3888 NE 50 FT - 82+00	89+00
R10A	RUNWAY	3	S	AC	3888 SW 50 FT - 82+00	89+00
005C	RUNWAY	1	S	AC	2500 8+50	10+00
006C	RUNWAY	1	S	ST	14166 0+00	8+50
007C	RUNWAY	1	S	AC	2500 92+00	93+50
T08A	TAXIWAY	1	P	PCC	14440	
T10A	TAXIWAY	1	P	PCC	3500 160 FT SW OF 13-31	260 FT NE OF 13-31
T12B	TAXIWAY	1	S	APC	9439 FEATURE T20A	FEATURE T13B
T14B	TAXIWAY	1	S		28782 FEATURE T13B	FEATURE T17B
T17B	TAXIWAY	ī	S	AC	17833 FEATURE T18B	FEATURE T14B
T18B	TAXIWAY	ī	S	AC		FEATURE T17B
A01B	APRON	1	S		20416	
A10B	APRON	ī	S		11183 BUILDINGS 195 196	AND 197
A31B	APRON	ī	S		24700	to an incredible of
		-	-	- 00		

	FY TO	O INSPE	CT : 19	94	NO. O	F SECTIONS : 50	
BRANCH	BRANCH	SECT.	PAVE.	SUT	SEC	FROM	TO
NUMBER	USE	NO.	RANK		AREA		
R01A	RUNWAY	2	P	PCC	5555	NW 50 FT - 10+00	20+00
RO1A	RUNWAY	3	P	PCC	5555	SE 50 FT - 10+00	20+00
R02C	RUNWAY	1.	P	PCC	71111	CENTER 80 FT-20+00	100+00
RO2C	RUNWAY	2	P	PCC	31111	NW 35 FT - 20+00	100+00
R02C	RUNWAY	3	P	PCC	31111	SE 35 FT - 20+00	100+00
RO3A	RUNWAY	3	P	PCC		SE 50 FT - 100+00	
R04A	RUNWAY	2	S	PCC		NE 50 FT - 10+00	
R04A	RUNWAY	3	S	PCC		SW 50 FT - 10+00	
R08C	RUNWAY	1	S	AC		CENTER 50 FT-40+00	
R08C	RUNWAY	3	S	AC	3333	SW 50 FT - 40+00	46+00
R09C	RUNWAY	3	S	AC	20000	SW 50 FT - 46+00	82+00
R11A	RUNWAY	1	S	PCC		CENTER 50 FT-89+00	
R11A	RUNWAY	2	S	PCC		NE 50 FT - 89+00	
R11A	RUNWAY	3	S	PCC	1666	SW 50 FT - 89+00	92+00
001C	RUNWAY	1	S	ST		111+50	
002C	RUNWAY	1	S	AC		110+00	111+50
003C	RUNWAY	1	S	AC		8+50	10+00
004C	RUNWAY	1	S	ST		0+00	
008C	RUNWAY	1	S	ST		93+50	
T03A	TAXIWAY	1	P		23333	FEATURE TO4A	
T04A	TAXIWAY	1	P	PCC		RUNWAY 04-22	
T05B	TAXIWAY		S	PCC	2961		
T06B	TAXIWAY	1	S	AC	7166		
T07C	TAXIWAY	1	S	AC	16041		
T09A	TAXIWAY	1	P	PCC	5333	260 FT NE OF 13-31	FEATURE T19A
T11A	TAXIWAY	1	P	PCC	20141	EDGE OF RW 04-22	160 FT SW OF 13-31
T13B	TAXIWAY	1	S	PCC		NW EDGE-RW 04-22	1+75
T16C	TAXIWAY	1	S	AC	2333		
T19A	TAXIWAY	1	P	PCC	3166	FEATURE TO9A	FEATURE T10A
T20A	TAXIWAY	1	P	PCC		FEATURE T19A	
T22B	TAXIWAY	1	S		1.0291		
A02B	APRON	1	P		180000		
A03B	APRON	1	S		17638		
A04B	APRON	1	S		71139		
A05B	APRON	1	S	PCC	9111		
A06B	APRON	1	S	PCC	1500		
A07B	APRON	1	S	PCC	4283		
A11B	APRON	1	S	PCC	4728		
A12B	APRON	1	S	PCC	5867		
A13B	APRON	1	S	PCC	1777		
A14B	APRON	1	S	PCC	1777		
A15B	APRON	1	S	PCC	5956		
A16B	APRON	ī	S	PCC	3831		
A18B	APRON	ī	S	PCC	4728		
A26B	APRON	ī	S	PCC	6600		
A27B	APRON	1	S	PCC	3719		
A28B	APRON	1	S	PCC	1833		
A29B	APRON	1	S	PCC	1556		

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A30B
     APRON
               1
                    S PCC 1844
ROAD
      APRON
               1
                      S
                          PCC 10583
  TOTAL NO. OF SECTION:
                           83
  SECT. NOT NEEDING INSPECTION:
                                0
  NO. OF MISSING VALUE: 7
  MISSING BR NO.
                 SEC. NO.
      T13B
                      2
      A08B
                       5
      A08B
                       4
      A08B
                       3
      A08B
                      2
      A08B
                      1
      A09B
                      2
         MINIMUM PCI TABLE
          P
               S
                   T
 RUNWAY
          0
               0
                   0
 APRON
          0
               0
                   0
 TAXIWAY
               0
          0
                   0
```

	RATE LIMIT		
RATE(PTS/YR)	YRS TO INSP		
GT 10	1		
6 - 10	1		
2 - 5	3		
LT 2	5		

SAMPCUR - CURRENT INSPECTION RESULTS

SAFFOOR - CORRENT INSPECTION RESULTS				
REPORT DATE- 07/20/8	9	PAVEMENT INS	PECTION	
AGENCY NUMBER =	1	CANNON AFB		
BRANCH NAME - RUNWAY 04-22 SLAB LENGTH - 25.0 LF BRANCH NUMBER - R01A SLAB WIDTH - 25.0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 80				
INSPECTION DATE - 06/02/89 PCI= 23 RATING= VERY POOR CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 4 NUMBER OF SAMPLES SURVEYED= 1 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.				
SAMPLE UNIT-3 (RANDO	M) S	AMPLE SIZE-	20 SLABS	SAMPLE PCI- 23
DISTRESS TYPE 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH 66 SMALL PATCH		JULADO		DEDUCT-VALUE 13.2 4.5 7.0 18.0 20.3 16.2 13.7 24.0 14.2 7.6
DISTRESS TYPE 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH 66 SMALL PATCH	SEVERITY HIGH MEDIUM MEDIUM LOW MEDIUM HIGH LOW MEDIUM HIGH HIGH MEDIUM HIGH	QUANTITY 4 SLABS 4 SLABS 80 SLABS 40 SLABS 12 SLABS 4 SLABS 16 SLABS 12 SLABS 12 SLABS 12 SLABS 12 SLABS	DENSITY-PCT 5.00 5.00 100.00 50.00 15.00 20.00 15.00 15.00 15.00	20.3 16.2 13.7 24.0 14.2 7.6
*** PERCENT OF D LOAD CLIMATE/DURABILITY OTHER	RELATED DIS	TRESSES = 38 TRESSES = 5	.86 PERCENT I	DEDUCT VALUES.

BRANCH NAME - RUNWA' BRANCH NUMBER - R01		SLAB LENGTH - 25.0 LF SLAB WIDTH - 25.0 LF NUMBER OF SLABS - 80
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SUR	02/89 SAFETY- ES IN SECTI	PCI=. 63 RATING= GOOD DRAINAGE- SHOULDERS- OVERALL-
RECOMMEND ALL SAMPLE		
SAMPLE UNIT-2 (RANDO	1)	SAMPLE SIZE- 20 SLABS SAMPLE PCI- 63
DISTRESS TYPE	SEVERIT	QUANTITY DENSITY-PCT DEDUCT-VALUE 20 SLABS 100.00 7.0 2 SLABS 10.00 16.8 10 SLABS 50.00 20.2
65 JT SEAL DAMAGE	MEDIUM	20 SLABS 100.00 7.0
67 LG PATCH/UTIL	MEDIUM	2 SLABS 10.00 16.8
63 LINEAR CR	LOW	10 SLABS 50.00 20.2
73 SHRINKAGE CR	N/A	2 SLABS 10.00 1.5
66 SMALL PATCH	LOW	7 SLABS 35.00 5.0
EXTRAPOLATED DISTRESS	QUANTITIE	FOR SECTION-
DISTRESS TYPE	SEVERTT	QUANTITY DENSITY-PCT DEDUCT-VALUE 80 SLABS 100.00 7.0 8 SLABS 10.00 16.8 40 SLABS 50.00 20.2 8 SLABS 10.00 1.5 28 SLABS 35.00 5.0
65 IT SEAL DAMAGE	MEDIUM	80 SLABS 100.00 7.0
67 LG PATCH/UTIL	MEDIUM	8 SLABS 10.00 16.8
63 LINEAR CR	1.00	40 SLABS 50.00 20.2
73 SHRINKAGE CR	N/A	8 SLABS 10.00 1.5
66 SMALL PATCH	LOW	28 SLABS 35.00 5.0
LOAD CLIMATE/DURABILITY OTHER BRANCH NAME - RUNWA' BRANCH NUMBER - ROL	RELATED DE RELATED DE RELATED DE PRESENTATION	ES BASED ON DISTRESS MECHANISM *** ISTRESSES - 40.00 PERCENT DEDUCT VALUES. ISTRESSES - 13.86 PERCENT DEDUCT VALUES. ISTRESSES - 46.14 PERCENT DEDUCT VALUES. SLAB LENGTH - 25.0 LF SLAB WIDTH - 25.0 LF
SECTION NUMBER - 3		NUMBER OF SLABS - 80
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMEND ALL SAMPLE	02/89 SAFETY- ES IN SECT VEYED=	1
SAMPLE UNIT-1 (RANDO	M)	SAMPLE SIZE- 20 SLABS SAMPLE PCI- 58
DISTRESS TYPE	SEVERIT	Y QUANTITY DENSITY-PCT DEDUCT-VALUE
67 LG PATCH/UTIL		1 SLABS 5.00 3.1
63 LINEAR CR		4 SLABS 20.00 13.7
72 SHATTERED SLAB	HIGH	1 SLABS 5.00 30.0
73 SHRINKAGE CR	N/A	4 SLABS 20.00 2.9

EXTRAPOLATED DISTRESS QU	JANTITIES	FOR SECT	CION-		
NTOWN TOO WAND	O DUED TOU	OII A NOTE	- 6777. 7	DENIGIOU DOM	DEDITOR HATTE
DISIRESS TIPE	SEVERITI	QUANTI	LII	DENSITY-PCI	DEDUCT-VALUE
67 LG PAICH/UILL	LOW	1.0	SLADS	2.00	3.1 12.7
63 LINEAR CR	TOM	16	SLABS	20.00	13./
72 SHATTERED SLAB	HIGH	1.6	SLABS	5.00	30.0
/3 SHRINKAGE CR	N/A	16	STYRS	20.00	2.9
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 67 LG PATCH/UTIL LOW 4 SLABS 5.00 3.1 63 LINEAR CR LOW 16 SLABS 20.00 13.7 72 SHATTERED SLAB HIGH 4 SLABS 5.00 30.0 73 SHRINKAGE CR N/A 16 SLABS 20.00 2.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = 87.93 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 12.07 PERCENT DEDUCT VALUES.					
DDANGI MAME DIMIAY				CTAR T	entoru oo o te
BRANCH NAME - RUNWAY	14-22			SLAB L	ENGIH - 20.0 LF
SECTION NUMBER - RUZU				SLAD W.	LDIH - ZU.U LF
SECTION NUMBER - I				NUMBER	OF SLABS - 1600
BRANCH NAME - RUNWAY 04-22 SLAB LENGTH - 20.0 LF BRANCH NUMBER - R02C SLAB WIDTH - 20.0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 1600 INSPECTION DATE - 06/02/89 PCI= 49 RATING= FAIR CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 80 NUMBER OF SAMPLES SURVEYED= 8 RECOMMENDED SAMPLES TO BE SURVEYED= 11 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 7.2					
SAMPLE UNIT-12 (RANDOM)		SAMPLE SI	ZE-	20 SLABS	SAMPLE PCI- 43
DISTRESS TYPE	SEVERITY	QUANT	TY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	HIGH	` 1	SLABS	5.00	5.1
74 JOINT SPALLING	HIGH	1	SLABS	5.00	13.2
65 JT SEAL DAMAGE	LOW	20	SLABS	100.00	2.0
67 LG PATCH/UTIL	LOW	8	SLABS	40.00	16.0
67 LG PATCH/UTIL	MEDIUM	5	SLABS	25.00	26.5
70 SCALING/CRAZING	LOW	16	SLABS	80.00	15.2
66 SMALL PATCH	HIGH	1	SLABS	5.00	6.3
75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH 66 SMALL PATCH	LOW	5	SLABS	25.00	3.2
SAMPLE UNIT-2 (RANDOM)					
DISTRESS TYPE	SEVERITY	OUANT	ſΤΥ	DENSITY-PCT	DEDUCT-VALUE
65 JT SEAL DAMAGE	LOW	•	SLABS	100.00	
67 LG PATCH/UTIL	HIGH		SLABS	5.00	
67 LG PATCH/UTIL	LOW		SLABS	5.00	
67 LG PATCH/UTIL	MEDIUM		SLABS		
70 SCALING/CRAZING	LOW		SLABS		15.2
73 SHRINKAGE CR	N/A		SLABS		
66 SMALL PATCH	LOW		SLABS		
66 SMALL PATCH	MEDIUM	3	SLABS	15.00	7.6

SAMPLE UNIT-22 (RANDOM)		SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 53
DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 66 SMALL PATCH	LOW MEDIUM	QUANTITY 20 SLABS 2 SLABS 15 SLABS 4 SLABS	DENSITY-PCT 100.00 10.00 75.00 20.00	DEDUCT-VALUE 2.0 6.0 45.5 2.2
SAMPLE UNIT-32 (RANDOM)		SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 54
DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH	LOW	20 SLABS	20.00	2.0
SAMPLE UNIT-42 (RANDOM)				
DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 66 SMALL PATCH	SEVERITY LOW MEDIUM LOW	QUANTITY 20 SLABS 15 SLABS 4 SLABS	DENSITY-PCT 100.00 75.00 20.00	DEDUCT-VALUE 2.0 45.5 2.2
SAMPLE UNIT-52 (RANDOM)		SAMPLE SIZE-		
DISTRESS TYPE 74 JOINT SPALLING 67 LG PATCH/UTIL 67 LG PATCH/UTIL 66 SMALL PATCH	HIGH LOW MEDIUM	1 SLABS 2 SLABS 6 SLABS	5.00 10.00 30.00	DEDUCT-VALUE 13.2 6.0 29.0 7.6
SAMPLE UNIT-62 (RANDOM)		SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 51
DISTRESS TYPE 67 LG PATCH/UTIL 66 SMALL PATCH	SEVERITY MEDIUM LOW		DENSITY-PCT 75.00 25.00	DEDUCT-VALUE 45.5 3.2
SAMPLE UNIT-72 (RANDOM)		SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 44
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 66 SMALL PATCH	LOW LOW MEDIUM LOW	1 SLABS 20 SLABS 1 SLABS 15 SLABS	5.00 100.00 5.00 75.00 25.00	2.2 2.0 3.1 45.5 3.2
EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-				
DISTRESS TYPE 75 CORNER SPALLING	SEVERITY	QUANTITY	DENSITY-PCT 0.62	DEDUCT-VALUE

74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE	HIGH LOW LOW	20 10 1200	SLABS SLABS SLABS	1.25 0.62 75.00	4.0 0.4 2.0 2.4
65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 70 SCALING/CRAZING 73 SHRINKAGE CR 66 SMALL PATCH 66 SMALL PATCH	HIGH LOW MEDIUM	10 200 890	SLABS SLABS SLABS	0.62 12.50 55.62	2.4 7.0 40.1
70 SCALING/CRAZING 73 SHRINKAGE CR	LOW N/A	440 10	SLABS SLABS	27.50 0.62	8.8 0.2
66 SMALL PATCH 66 SMALL PATCH	LOW MEDIUM	470 30	SLABS SLABS	29.37 1.87	3.8 1.0
*** PERCENT OF DED LOAD R CLIMATE/DURABILITY R OTHER R	ELATED DI ELATED DI ELATED DI	STRESSES STRESSES STRESSES	= 2. = 97.	TRESS MECHANI .00 PERCENT D .80 PERCENT D .20 PERCENT D	EDUCT VALUES. EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - RUNWAY BRANCH NUMBER - RO2C SECTION NUMBER - 2	04-22			SLAB LE SLAB WI NUMBER	NGTH - 17.5 LF DTH - 20.0 LF OF SLABS - 800
INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF P	/89 AFETY- IN SECTI YED= BE SURVEY CI BETWEE	PCI= 89 DRAINAGE ON= ED= N RANDOM	RAT	ring= excell shoulders -	ENT OVERALL- 40 4 26
SAMPLE UNIT-14 (RANDOM)		SAMPLE SI			
DISTRESS TYPE 63 LINEAR CR 66 SMALL PATCH	SEVERITY LOW LOW	QUANTI 1 7	TY SLABS SLABS	DENSITY-PCT 5.00 35.00	DEDUCT-VALUE 4.9 5.0
SAMPLE UNIT-24 (RANDOM)					
DISTRESS TYPE 66 SMALL PATCH	LOW	6	SLABS	DENSITY-PCT 30.00	4.0
SAMPLE UNIT-34 (RANDOM)					
DISTRESS TYPE 66 SMALL PATCH	LOW	4	SLABS	DENSITY-PCT 20.00	2.2
SAMPLE UNIT-4 (RANDOM)					
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL	LOW MEDIUM MEDIUM	1 3 20	SLABS SLABS SLABS	5.00 15.00 100.00	1.9 9.7 7.0

70 SCALING/CRAZING	LOW	12 S	LABS	60.00	13.5
66 SMALL PATCH	LOW	8 S	LABS	40.00	5.7
70 SCALING/CRAZING 66 SMALL PATCH 66 SMALL PATCH	MEDIUM	1 S	LABS	5.00	2.7
EXTRAPOLATED DISTRESS	-				
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING	SEVERITY	QUANTIT	Y :	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	LOW	10 S	LABS	1.25	0.4
75 CORNER SPALLING	MEDIUM	30 S	LABS	3.75	2.1
67 LG PATCH/UTIL	LOW	10 S	LABS	1.25	0.9
63 LINEAR CR	LOW	10 S	LABS	1.25	1.2
70 SCALING/CRAZING	LOW	120 S	LABS	15.00	5.6
66 SMALL PATCH	LOW	250 S	LABS	31.25	4.2
67 LG PATCH/UTIL 63 LINEAR CR 70 SCALING/CRAZING 66 SMALL PATCH 66 SMALL PATCH	MEDIUM	10 S	LABS	1.25	0.6
*** PERCENT OF DI					
LOAD					
CLIMATE/DURABILITY	RELATED DIS	TRESSES -	31.	82 PERCENT D	EDUCT VALUES.
OTHER					
BRANCH NAME - RUNWAY BRANCH NUMBER - RO20	7 04-22			SLAB LE	NGTH - 17.5 LF
BRANCH NUMBER - RO20				SLAB WI	DTH - 20.0 LF
SECTION NUMBER - 3				NUMBER	OF SLABS - 800
W					
INSPECTION DATE - 06/0	02/89 P	CI= 87	RAT	ING= EXCELL	ENT
CONDITION- RIDING-	SAFETY-	DRAINAGE-	RAT:	ING= EXCELL	ENT OVERALL-
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE	SAFETY- ES IN SECTIO	DRAINAGE-	RAT: SI	ING= EXCELL	ENT OVERALL- 40
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV	SAFETY- ES IN SECTIO VEYED=	DRAINAGE- N=	RAT:	ING= EXCELL	ENT OVERALL- 40 3
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO	SAFETY- ES IN SECTIO VEYED=) BE SURVEYE	DRAINAGE - N= D=	SI	ING= EXCELL HOULDERS -	ENT OVERALL- 40 3 22
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV	SAFETY- ES IN SECTIO VEYED=) BE SURVEYE	DRAINAGE - N= D=	SI	ING= EXCELL HOULDERS -	ENT OVERALL- 40 3 22
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	SAFETY- ES IN SECTIO VEYED= D BE SURVEYE PCI BETWEEN	DRAINAGE- N= D= RANDOM U	Si NITS	ING= EXCELL HOULDERS - SURVEYED=	ENT OVERALL- 40 3 22 7.9
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDOM	SAFETY- ES IN SECTIO VEYED= D BE SURVEYE PCI BETWEEN (1) S	DRAINAGE- N= D= RANDOM U AMPLE SIZ	SI NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDOM	SAFETY- ES IN SECTIO VEYED= D BE SURVEYE PCI BETWEEN (1) S	DRAINAGE- N= D= RANDOM U AMPLE SIZ	SI NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDOM	SAFETY- ES IN SECTIO VEYED= D BE SURVEYE PCI BETWEEN (1) S	DRAINAGE- N= D= RANDOM U AMPLE SIZ	SI NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDOM	SAFETY- ES IN SECTIO VEYED= D BE SURVEYE PCI BETWEEN (1) S	DRAINAGE- N= D= RANDOM U AMPLE SIZ	SI NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDOM	SAFETY- ES IN SECTIO VEYED= D BE SURVEYE PCI BETWEEN (1) S	DRAINAGE- N= D= RANDOM U AMPLE SIZ	SI NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDOM	SAFETY- ES IN SECTIO VEYED= D BE SURVEYE PCI BETWEEN (1) S	DRAINAGE- N= D= RANDOM U AMPLE SIZ	SI NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW LOW LOW LOW LOW	DEAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 3 S 10 S 10 S	NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 15.00 50.00 50.00	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW LOW LOW LOW LOW	DEAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 3 S 10 S 10 S	NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 15.00 50.00 50.00	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDOM DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDOM	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW LOW LOW LOW LOW LOW SERVERITY LOW	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 3 S 10 S 10 S	NITS :	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 15.00 50.00 50.00 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDON DISTRESS TYPE	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW LOW LOW LOW LOW SEVERITY SEVERITY	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 3 S 10 S 10 S	NITS (E- : LABS LABS LABS LABS	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 15.00 50.00 50.00 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDON DISTRESS TYPE	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW LOW LOW LOW LOW SEVERITY SEVERITY	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 3 S 10 S 10 S	NITS (E- : LABS LABS LABS LABS	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 15.00 50.00 50.00 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDON DISTRESS TYPE 75 CORNER SPALLING 66 SMALL PATCH	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 3 S 10 S 10 S AMPLE SIZ QUANTIT 2 S 11 S	INITS STATES TABS LABS LABS LABS LABS LABS LABS LABS L	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 50.00 50.00 50.00 20 SLABS DENSITY-PCT 10.00 55.00	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87 DEDUCT-VALUE 9.0 7.6
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDON DISTRESS TYPE 75 CORNER SPALLING 66 SMALL PATCH	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 3 S 10 S 10 S 20 S 40 S	INITS STATES TABS LABS LABS LABS LABS LABS LABS LABS L	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 15.00 50.00 50.00 20 SLABS DENSITY-PCT 10.00 55.00	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87 DEDUCT-VALUE 9.0 7.6
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDON DISTRESS TYPE 75 CORNER SPALLING 66 SMALL PATCH	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 3 S 10 S 10 S 20 S 40 S	INITS STATES TABS LABS LABS LABS LABS LABS LABS LABS L	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 15.00 50.00 50.00 20 SLABS DENSITY-PCT 10.00 55.00	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87 DEDUCT-VALUE 9.0 7.6
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDON DISTRESS TYPE 75 CORNER SPALLING 66 SMALL PATCH SAMPLE UNIT-7 (RANDON	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 10 S 10 S 10 S AMPLE SIZ QUANTIT 2 S 11 S AMPLE SIZ	INITS STATES TO	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 50.00 50.00 50.00 50.00 50.00 50.00 50.00 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87 DEDUCT-VALUE 9.0 7.6 SAMPLE PCI- 95
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDON DISTRESS TYPE 75 CORNER SPALLING 66 SMALL PATCH SAMPLE UNIT-7 (RANDON	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 10 S 10 S 10 S AMPLE SIZ QUANTIT 2 S 11 S AMPLE SIZ	INITS STATES TO	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 50.00 50.00 50.00 50.00 50.00 50.00 50.00 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87 DEDUCT-VALUE 9.0 7.6 SAMPLE PCI- 95
CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-17 (RANDON DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH SAMPLE UNIT-27 (RANDON DISTRESS TYPE 75 CORNER SPALLING 66 SMALL PATCH	SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN SEVERITY LOW	DRAINAGE- N= RANDOM U AMPLE SIZ QUANTIT 20 S 10 S 10 S 10 S AMPLE SIZ QUANTIT 2 S 11 S AMPLE SIZ	INITS STATES TO	ING= EXCELL HOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 50.00 50.00 50.00 50.00 50.00 50.00 50.00 20 SLABS	ENT OVERALL- 40 3 22 7.9 SAMPLE PCI- 79 DEDUCT-VALUE 2.0 8.0 12.2 7.0 SAMPLE PCI- 87 DEDUCT-VALUE 9.0 7.6 SAMPLE PCI- 95

EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION-		
DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH	HIGH LOW LOW LOW	27 SLABS 267 SLABS 53 SLABS 133 SLABS 320 SLABS	3.37 33.37 6.62 16.62 40.00	3.6 2.0 4.1 6.0 5.7
		BASED ON DISTRES		
LOAD	RELATED DIS	STRESSES = .00	PERCENT DEDU	CT VALUES.
CLIMATE/DURABILITY				
OTHER	KELAIED DIS	OINESSES = 90.00	PERCENI DEDU	OI VALUES.
BRANCH NAME - RUNWA	Y 04-22			TH - 25.0 LF
BRANCH NUMBER - RO3	A		SLAB WIDTH	I - 25.0 LF
			NUMBER OF	SLABS - 80
INSPECTION DATE - 06/				
CONDITION- RIDING-	SAFETY-	DRAINAGE- SHOU	LDERS - OV	'ERALL-
TOTAL NUMBER OF SAMPL	ES IN SECTIO)N=		4
NUMBER OF SAMPLES SUR				1
RECOMMEND ALL SAMPLE		SURVEYED.		
SAMPLE UNIT-4 (RANDO				
DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING	SEVERITY	QUANTITY DEN	SITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	LOW	1 SLABS	5.00	1.9
74 JOINT SPALLING	LOW	1 SLABS	5.00	2.2
74 JOINT SPALLING	MEDIUM	3 SLABS	15.00	10.9
63 LINEAR CR	LOW	1 SLABS	5.00	4.9
63 LINEAR CR	MEDIUM	1 SLABS	5.00	11.6
73 SHRINKAGE CR	N/A	3 SLABS	15.00	2.1
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION-		
DISTRESS TYPE	SEVERITY	QUANTITY DEN	SITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING		4 SLABS	5.00	1.9
			5.00	2.2
74 JOINT SPALLING 74 JOINT SPALLING	MEDIUM	12 SLABS	15.00	10.9
63 LINEAR CR	LOW	4 SLABS	5.00	4.9
63 LINEAR CR	MEDIUM		5.00	11.6
73 SHRINKAGE CR	N/A	12 SLABS	15.00	2.1
		BASED ON DISTRES		
LOAD	RELATED DIS	TRESSES = 49.11	PERCENT DEDU	CT VALUES.
LOAD CLIMATE/DURABILITY	RELATED DIS	TRESSES = 49.11	PERCENT DEDU PERCENT DEDU	CT VALUES.

BRANCH NAME - RUNWAY BRANCH NUMBER - RO3A SECTION NUMBER - 2	04-22			SLAB LE SLAB WI NUMBER	CNGTH - 25.0 LF DTH - 25.0 LF OF SLABS - 80
INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVI RECOMMEND ALL SAMPLE UN	SAFETY- D S IN SECTION EYED= NITS TO BE S	DRAINAGI I= SURVEYEI	E- S	TING= VERY G	OOD OVERALL- 4 1
SAMPLE UNIT-1 (RANDOM)) SA	MPLE SI			
DISTRESS TYPE 74 JOINT SPALLING 67 LG PATCH/UTIL 66 SMALL PATCH EXTRAPOLATED DISTRESS (DENSITY-PCT 5.00 100.00 5.00	DEDUCT-VALUE 2.2 22.3 0.6
DISTRESS TYPE 74 JOINT SPALLING 67 LG PATCH/UTIL 66 SMALL PATCH *** PERCENT OF DEI LOAD CLIMATE/DURABILITY OTHER	LOW LOW LOW DUCT VALUES RELATED DIST	BASED (CRESSES TRESSES	SLABS SLABS SLABS ON DIST	5.00 100.00 5.00 TRESS MECHANI .00 PERCENT I	2.2 22.3 0.6 SSM *** DEDUCT VALUES. DEDUCT VALUES.
BRANCH NAME - RUNWAY BRANCH NUMBER - RO3A SECTION NUMBER - 3	04-22			SLAB WI NUMBER	ENGTH - 25.0 LF IDTH - 25.0 LF OF SLABS - 80
INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVI RECOMMEND ALL SAMPLE UN	SAFETY- D S IN SECTION EYED=	RAINAGI I=	RA' E- S	ring- exceli	
SAMPLE UNIT-2 (RANDOM)) SA	MPLE S	IZE-	20 SLABS	SAMPLE PCI- 88
DISTRESS TYPE 63 LINEAR CR 66 SMALL PATCH	SEVERITY MEDIUM LOW	QUANT:	ITY SLABS SLABS	DENSITY-PCT 5.00 5.00	DEDUCT-VALUE 11.6 0.6
EXTRAPOLATED DISTRESS (
DISTRESS TYPE 63 LINEAR CR 66 SMALL PATCH	MEDIUM	4	SLABS	5.00	11.6

LOAD RELATED DISTRESSES = 95.08 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
OTHER RELATED DISTRESSES = 4.92 PERCENT DEDUCT VALUES. BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF SECTION NUMBER - 1 NUMBER OF SLABS - 96 _____ INSPECTION DATE - 06/02/89 PCI= 19 RATING= VERY POOR CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-TOTAL NUMBER OF SAMPLES IN SECTION= 5 2 NUMBER OF SAMPLES SURVEYED-RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. _____ SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 15
 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 62 CORNER BR
 LOW
 2 SLABS
 10.00
 8.0

 75 CORNER SPALLING
 HIGH
 1 SLABS
 5.00
 5.1

 75 CORNER SPALLING
 LOW
 3 SLABS
 15.00
 5.5

 64 DURABILITY CR
 HIGH
 1 SLABS
 5.00
 8.3

 74 JOINT SPALLING
 LOW
 2 SLABS
 10.00
 3.5

 74 JOINT SPALLING
 MEDIUM
 2 SLABS
 10.00
 8.0

 65 JT SEAL DAMAGE
 HIGH
 20 SLABS
 100.00
 12.0

 67 LG PATCH/UTIL
 MEDIUM
 5 SLABS
 25.00
 26.5

 63 LINEAR CR
 HIGH
 3 SLABS
 15.00
 33.1

 63 LINEAR CR
 MEDIUM
 1 SLABS
 5.00
 31.6

 72 SHATTERED SLAB
 HIGH
 1 SLABS
 5.00
 30.0

 73 SHRINKAGE CR
 N/A
 13 SLABS
 65.00
 10.0
 -----SAMPLE UNIT-5 (RANDOM) SAMPLE SIZE- 16 SLABS SAMPLE PCI- 22
 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 62 CORNER BR
 MEDIUM
 1 SLABS
 6.25
 10.3

 75 CORNER SPALLING
 HIGH
 1 SLABS
 6.25
 6.1

 75 CORNER SPALLING
 LOW
 2 SLABS
 12.50
 4.7

 75 CORNER SPALLING
 MEDIUM
 2 SLABS
 12.50
 8.5

 74 JOINT SPALLING
 HIGH
 2 SLABS
 12.50
 23.4

 74 JOINT SPALLING
 MEDIUM
 4 SLABS
 25.00
 16.0

 65 JT SEAL DAMAGE
 HIGH
 16 SLABS
 100.00
 12.0

 67 LG PATCH/UTIL
 HIGH
 1 SLABS
 6.25
 21.0

 67 LG PATCH/UTIL
 LOW
 4 SLABS
 25.00
 12.0

 63 LINEAR CR
 LOW
 2 SLABS
 12.50
 10.0

 73 SHRINKAGE CR
 N/A
 7 SLABS
 43.75
 6.3

 66 SMALL PATCH
 MEDIUM
 1 SLABS
 6.25
 3.3

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECT	CION-		
DISTRESS TYPE 62 CORNER BR 62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 75 CORNER SPALLING 64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH	SEVERITY	OUANT	TTY	DENSITY-PCT	DEDUCT-VALUE
62 CORNER BR	LOW	5	SLABS	5.20	4.2
62 CORNER BR	MEDIUM	, 3	SLABS	3.12	5.1
75 CORNER SPALLING	HIGH	5	SLABS	5.20	5.2
75 CORNER SPALLING	LOW	13	SLABS	13.54	5.0
75 CORNER SPALLING	MEDIUM	5	SLABS	5.20	3.5
64 DURABILITY CR	HIGH	3	SLABS	3.12	5.5
74 JOINT SPALLING	HIGH	5	SLABS	5.20	13.6
74 JOINT SPALLING	LOW	13	SLABS	13.54	4.4
74 JOINT SPALLING	MEDIUM	16	SLABS	16.66	11.7
65 JT SEAL DAMAGE	HIGH	96	SLABS	100.00	12.0
67 LG PATCH/UTIL	HIGH	3	SLABS	3.12	11.9
67 LG PATCH/UTIL	LOW	11	SLABS	11.45	6.5
67 LG PATCH/UTIL	MEDIUM	13	SLABS	13.54	19.3
63 LINEAR CR	HIGH	8	SLABS	8.33	22.9
63 LINEAR CR	LOW	5	SLABS	5.20	5.0
63 LINEAR CR	MEDIUM	3	SLABS	3.12	7.8
72 SHATTERED SLAB	HIGH	3	SLABS	3.12	16.2
73 SHRINKAGE CR	N/A	53	SLABS	55.20	8.0
66 SMALL PATCH	LOW	8	SLABS	8.33	1.0
66 SMALL PATCH	MEDIUM	3	SLABS	3.12	1.7
*** PERCENT OF DE LOAD CLIMATE/DURABILITY OTHER	RELATED DIS RELATED DIS	STRESSES STRESSES STRESSES	= 35. = 10. = 53.	.89 PERCENT D	EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - RUNWAY BRANCH NUMBER - RO4A SECTION NUMBER - 2	7 13-31			SLAB LE	NGTH - 12.5 LF DTH - 12.5 LF
SECTION NUMBER - 2				NUMBER	OF SLABS - 96
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE U	SAFETY- ES IN SECTIO VEYED** UNITS TO BE	DRAINAGI)N= SURVEYEI	RAT	ring- fair shoulders-	OVERALL- 5 2
SAMPLE UNIT-2 (RANDOM				20 SLABS	
DISTRESS TYPE	SEVERITY	OUANT	(TY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	LOW			20.00	7.0
74 JOINT SPALLING	HTGH		SLABS	5.00	13.2
74 JOINT SPALLING 65 JT SEAL DAMAGE	LOW		SLABS	40.00	9.5
65 JT SEAL DAMAGE	HIGH	20	SLABS	100.00	12.0
67 LG PATCH/UTIL	LOW		SLABS		3.1
63 LINEAR CR	UTAU				
(0 7 TIVELE AD	HIGH	2	SLABS	10.00	26.0
63 LINEAR CR	MEDIUM	1	SLABS SLABS		26.0 11.6

SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 56	66 SMALL PATCH	LOW	3	SLABS	15.00	1.6
To conner spalling	SAMPLE UNIT-4 (RANDO	M) S	SAMPLE SI	ZE-	20 SLABS	SAMPLE PCI- 56
To conner spalling	DISTRESS TYPE	SEVERITY	OUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 2 SLABS 2.08 2.4 75 CORNER SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 19 SLABS 2.08 6.7 74 JOINT SPALLING LOW 19 SLABS 19.79 5.8 65 JT SEAL DAMAGE HIGH 96 SLABS 100.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 22.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 63 LINEAR CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. BERANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BREANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BREANCH NUMBER OF SAMPLES IN SECTION PARINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION PARINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION PARINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION SAMPLE SIZE 20 SLABS SAMPLE PCI - 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5	75 CORNER SPALLING	HIGH	1	SLABS	5.00	5.1
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 2 SLABS 2.08 2.4 75 CORNER SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 19 SLABS 2.08 6.7 74 JOINT SPALLING LOW 19 SLABS 19.79 5.8 65 JT SEAL DAMAGE HIGH 96 SLABS 100.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 22.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 63 LINEAR CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. BERANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BREANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BREANCH NUMBER OF SAMPLES IN SECTION PARINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION PARINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION PARINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION SAMPLE SIZE 20 SLABS SAMPLE PCI - 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5	65 JT SEAL DAMAGE	HIGH	20	SLABS	100.00	12.0
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 2 SLABS 2.08 2.4 75 CORNER SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 19 SLABS 1.09 6.7 74 JOINT SPALLING LOW 19 SLABS 10.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 2.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 66 SLABS 7.29 20.9 66 SLABS 7.29 20.9 66 SLABS 7.29 20.9 63 LINEAR CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES — 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES — 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES — 51.03 PERCENT DEDUCT VALUES. BEANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NAME - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER OF SAMPLES IN SECTION— 5 NUMBER OF SAMPLES SURVEYED— 5 RECOMDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION— 5 NUMBER OF SAMPLES SURVEYED— 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2	67 LG PATCH/UTIL	LOW	8	SLABS	40.00	16.0
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 2 SLABS 2.08 2.4 75 CORNER SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 19 SLABS 1.09 6.7 74 JOINT SPALLING LOW 19 SLABS 10.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 2.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 66 SLABS 7.29 20.9 66 SLABS 7.29 20.9 66 SLABS 7.29 20.9 63 LINEAR CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES — 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES — 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES — 51.03 PERCENT DEDUCT VALUES. BEANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NAME - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER OF SAMPLES IN SECTION— 5 NUMBER OF SAMPLES SURVEYED— 5 RECOMDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION— 5 NUMBER OF SAMPLES SURVEYED— 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2	63 LINEAR CR	HIGH	1	SLABS	5.00	16.2
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 2 SLABS 2.08 2.4 75 CORNER SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 19 SLABS 1.09 6.7 74 JOINT SPALLING LOW 19 SLABS 10.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 2.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 66 SLABS 7.29 20.9 66 SLABS 7.29 20.9 66 SLABS 7.29 20.9 63 LINEAR CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES — 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES — 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES — 51.03 PERCENT DEDUCT VALUES. BEANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NAME - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER - RO4A SLAB WITH - 12.5 LF BRANCH NUMBER OF SAMPLES IN SECTION— 5 NUMBER OF SAMPLES SURVEYED— 5 RECOMDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION— 5 NUMBER OF SAMPLES SURVEYED— 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2	73 SHRINKAGE CR	N/A	11	SLABS	55.00	8.0
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 2 SLABS 2.08 2.4 75 CORNER SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 19 SLABS 2.08 6.7 74 JOINT SPALLING LOW 19 SLABS 10.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 2.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 66 SLABS 7.29 20.9 66 SLABS 7.29 20.9 67 LG PATCH/UTIL LOW 22 SLABS 2.08 5.2 73 SHRINKAGE CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. BERANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER OF SAMPLES IN SECTION DATE - 06/02/89 PCI = 56 RATING GOOD CONDITION - RIDING SAFETY DRAINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION 5 NUMBER OF SAMPLES SURVEYED 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2	66 SMALL PATCH	нтен	1	SLABS	5.00	6.3
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 2 SLABS 2.08 2.4 75 CORNER SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING LOW 19 SLABS 2.08 6.7 74 JOINT SPALLING LOW 19 SLABS 10.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 2.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 66 SLABS 7.29 20.9 66 SLABS 7.29 20.9 67 LG PATCH/UTIL LOW 22 SLABS 2.08 5.2 73 SHRINKAGE CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. BERANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER OF SAMPLES IN SECTION DATE - 06/02/89 PCI = 56 RATING GOOD CONDITION - RIDING SAFETY DRAINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION 5 NUMBER OF SAMPLES SURVEYED 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING MEDIUM 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2	66 SMALL PATCH	I.OW	4	SLABS	20.00	2.2
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 2 SLABS 2.08 2.4 75 CORNER SPALLING LOW 10 SLABS 10.41 3.8 74 JOINT SPALLING HIGH 2 SLABS 2.08 6.7 74 JOINT SPALLING LOW 19 SLABS 19.79 5.8 65 JT SEAL DAMAGE HIGH 96 SLABS 100.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 22.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 63 LINEAR CR HIGH 7 SLABS 2.08 5.2 64 SMALL PATCH HIGH 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. BERANCH NAME - RUNWAY 13-31 SLAB WIDTH - 12.5 LF BERANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BERANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF BERANCH NUMBER - SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION- 5 NUMBER OF SAMPLES IN SECTION- 5 NUMBER OF SAMPLE UNITS TO BE SURVEYED. DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING HIGH 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.3						
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74 JOINT SPALLING HIGH 2 SLABS 2.08 6.7 74 JOINT SPALLING LOW 19 SLABS 19.79 5.8 65 JT SEAL DAMAGE HIGH 96 SLABS 100.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 22.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 63 LINEAR CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. SECTION NUMBER - RO4A SLAB WIDTH - 12.5 LF BRANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF SECTION NUMBER - 3 NUMBER OF SLABS - 96 INSPECTION DATE - 06/02/89 PCI = 56 RATING = GOOD CONDITION - RIDING - SAFETY DRAINAGE - SHOULDERS - OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION = 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. DISTRESS TYPE SEVERITY QUANTITY DENSITY PCT DEDUCT VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5	75 CORNER SPALLING	LOW	10	SLABS	10.41	3.8
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65 JT SEAL DAMAGE HIGH 96 SLABS 100.00 12.0 67 LG PATCH/UTIL LOW 22 SLABS 22.91 11.1 63 LINEAR CR HIGH 7 SLABS 7.29 20.9 63 LINEAR CR MEDIUM 2 SLABS 2.08 5.2 73 SHRINKAGE CR N/A 31 SLABS 32.29 4.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 **** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES - 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES - 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES - 51.03 PERCENT DEDUCT VALUES. BERANCH NAME - RUNWAY 13-31 SLAB WIDTH - 12.5 LF BERANCH NUMBER - RO4A SLAB WIDTH - 12.5 LF SECTION NUMBER - 3 NUMBER OF SLABS - 96 INSPECTION DATE - 06/02/89 PCI - 56 RATING GOOD CONDITION - RIDING - SAFETY - DRAINAGE - SHOULDERS - OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION - 5 NUMBER OF SAMPLES SURVEYED - 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING HIGH 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.3	74 JOINT SPALLING	LOW	19	SLABS	19.79	5.8
66 SMALL PATCH HIGH 2 SLABS 2.08 3.5 66 SMALL PATCH LOW 17 SLABS 17.70 1.9 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES - 33.55 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES - 15.42 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES - 51.03 PERCENT DEDUCT VALUES. BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NUMBER - R04A SLAB WIDTH - 12.5 LF SECTION NUMBER - 3 NUMBER OF SLABS - 96 INSPECTION DATE - 06/02/89 PCI - 56 RATING GOOD CONDITION RIDING SAFETY DRAINAGE SHOULDERS OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION 5 NUMBER OF SAMPLES SURVEYED 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE - 20 SLABS SAMPLE PCI - 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5	CE TO COAT DAMAGE	UTOU	0.6	CTADC	100 00	10 0
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OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NUMBER - R04A SLAB WIDTH - 12.5 LF SECTION NUMBER - 3 NUMBER OF SLABS - 96 INSPECTION DATE - 06/02/89 PCI= 56 RATING= GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 5 NUMBER OF SAMPLES SURVEYED= 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5	LOAD	RELATED DIS	TRESSES	== 33 .	.55 PERCENT D	EDUCT VALUES.
OTHER RELATED DISTRESSES = 51.03 PERCENT DEDUCT VALUES. BRANCH NAME - RUNWAY 13-31 SLAB LENGTH - 12.5 LF BRANCH NUMBER - R04A SLAB WIDTH - 12.5 LF SECTION NUMBER - 3 NUMBER OF SLABS - 96 INSPECTION DATE - 06/02/89 PCI= 56 RATING= GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 5 NUMBER OF SAMPLES SURVEYED= 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2	CLIMATE/DURABILITY	RELATED DIS	TRESSES	= 15.	.42 PERCENT D	EDUCT VALUES.
SECTION NUMBER - 3 NUMBER OF SLABS - 96 INSPECTION DATE - 06/02/89 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION- 5 NUMBER OF SAMPLES SURVEYED- 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5						
SECTION NUMBER - 3 NUMBER OF SLABS - 96 INSPECTION DATE - 06/02/89 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION- 5 NUMBER OF SAMPLES SURVEYED- 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5						
SECTION NUMBER - 3 NUMBER OF SLABS - 96 INSPECTION DATE - 06/02/89 CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION- 5 NUMBER OF SAMPLES SURVEYED- 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5	BRANCH NAME - RUNWAY	Y 13-31			SLAB LE	NGTH - 12.5 LF
INSPECTION DATE - 06/02/89 PCI= 56 RATING= GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 5 NUMBER OF SAMPLES SURVEYED= 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5	BRANCH NUMBER - RO4	A			SLAB WI	DTH - 12.5 LF
INSPECTION DATE - 06/02/89 PCI= 56 RATING= GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 5 NUMBER OF SAMPLES SURVEYED= 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5						
CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION- 5 NUMBER OF SAMPLES SURVEYED- 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING HIGH 1 SLABS 5.00 3.5						
TOTAL NUMBER OF SAMPLES IN SECTION— 5 NUMBER OF SAMPLES SURVEYED— 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5	CONDITION- RIDING-	SAFETY-	DRATNAGE	. KA	SHOULDERS-	OVERALL.
NUMBER OF SAMPLES SURVEYED= 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5	TOTAL NUMBER OF SAMPL	ES IN SECTIO	N=		JII O DD DIKO	5
RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5						
SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 66 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH SLABS S.00 S.1 75 CORNER SPALLING MEDIUM SLABS S.00 3.3 74 JOINT SPALLING HIGH SLABS S.00 3.3 74 JOINT SPALLING HIGH SLABS SAMPLE PCI- 66			SURVEYED	1_		-
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5						
75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5	SAMPLE UNIT-1 (RANDO	M) S	SAMPLE SI	ZE-	20 SLABS	SAMPLE PCI- 66
75 CORNER SPALLING HIGH 1 SLABS 5.00 5.1 75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5	DISTRESS TYPE	SEVERITY	QUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING MEDIUM 1 SLABS 5.00 3.3 74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5	75 CORNER SPALLING	HIGH	1	SLABS	5.00	5.1
74 JOINT SPALLING HIGH 1 SLABS 5.00 13.2 74 JOINT SPALLING LOW 2 SLABS 10.00 3.5	75 CORNER SPALLING	MEDIUM	1	SLABS	5.00	3.3
74 JOINT SPALLING LOW 2 SLABS 10.00 3.5						
	74 JOINT SPALLING	LOW	2	SLABS	10.00	3.5

63 LINEAR CR					
66 SMALL PATCH					
66 SMALL PATCH	MEDIUM	1	SLABS	5.00	2.7
SAMPLE UNIT-4 (RANDOM)		SAMPLE S	IZE-	20 SLABS	SAMPLE PCI- 46
DISTRESS TYPE	SEVERITY	Y OUANT	ITY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	MEDIUM	` 1	SLABS	5.00	3.3
74 JOINT SPALLING	T.OW	3	SLABS	15.00	4.7
74 IOINT SPALLING	MEDITIM	1	SLABS	5 00	4.5
65 IT SEAT DAMAGE	HIGH	20	STARS	100.00	12.0
75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 72 SHATTERED SLAB 73 SHRINKAGE CR	IOU	1	CTARC	5.00	4.5 12.0 3.1
70 CHATTEDED CLAP	LOW UTCU	1	סם א זס	5.00	30.0
/2 SHATTERED SLAD	NIGH NI/A	1	STADS	30.00	30.0
/3 SHRINKAGE OR	IV/A	0	SLADS	10.00	4.2
66 SMALL PATCH	LOW	2	STABS	10.00	1.1
EXTRAPOLATED DISTRESS C	UANTITIES	FOR SEC	rion-		
DISTRESS TYPE	SEVERITY	Y QUANT:	LTY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	HIGH	2	SLABS	2.08	2.4
75 CORNER SPALLING					
74 JOINT SPALLING	HIGH	2	SLABS	2.08	6.7
75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH 66 SMALL PATCH	LOW	12	SLABS	12.50	4.1
74 JOINT SPALLING	MEDIUM	2	SLABS	2.08	2.1
65 JT SEAL DAMAGE	HIGH	96	SLABS	100.00	12.0
67 LG PATCH/UTIL	I.OW	2	SLABS	2.08	1.5
63 ITNEAR CR	I OU	7	STARS	7 29	6.6
72 CHATTEDED CLAR	HICH	,	STARS	2.08	2 9
72 SHATTERED SEAD	NI /A	1/2	STARC	1/, 58	2.0
66 CMAIL DATCU	IOU	7	CIADO	7 20	0.0
66 SMALL PATCH	TOM	,	STADS	7.47	1.1
66 SMALL PAICH	MEDIOM	2	SLADS	2.08	1.1
*** PERCENT OF DEC LOAD R CLIMATE/DURABILITY R	ELATED DI	STRESSES STRESSES	- 20 - 26	.74 PERCENT I	DEDUCT VALUES. DEDUCT VALUES.
OTHER R	ELATED DI	ISTRESSES	≖ 53.	.06 PERCENT I	DEDUCT VALUES.
BRANCH NAME - RUNWAY	13-31			SECTION	LENGTH - 700 LF
BRANCH NUMBER - RO5A				SECTION	WIDTH - 50 LF
SECTION NUMBER - 1				SECTION	N AREA - 3888 SY
				• • • • • • • • • • • • •	•••••
INSPECTION DATE - 06/02	/89	PCI- 76	RA.	ring- very (GOOD
CONDITION- RIDING- S	AFETY-	DRAINAGI	3- 5	SHOULDERS -	OVERALL-
TOTAL NUMBER OF SAMPLES	IN SECTI	ON-			7
NUMBER OF SAMPLES SURVE	YED-				2
RECOMMENDED SAMPLES TO	BE SURVEY	ED=			7
STANDARD DEVIATION OF P			UNITS	SURVEYED=	10.6
SAMPLE UNIT-3 (RANDOM)		SAMPLE S	ZE- 50	000 SF	SAMPLE PCI- 68

DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR	SEVERITY LOW LOW	QUANTITY 160 SF 55 LF	DENSITY-PCT 3.20 1.10	DEDUCT-VALUE 32.0 5.7
SAMPLE UNIT-7 (RANDON	1) S.	AMPLE SIZE-	5000 SF	SAMPLE PCI- 83
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR	SEVERITY LOW LOW	QUANTITY 30 SF 243 LF	DENSITY-PCT 0.60 4.86	DEDUCT-VALUE 15.7 13.7
EXTRAPOLATED DISTRESS				
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR	SEVERITY LOW LOW	QUANTITY 665 SF 1043 LF	DENSITY-PCT 1.90 2.98	DEDUCT-VALUE 26.5 9.9
CLIMATE/DURABILITY OTHER	RELATED DISTRELATED DISTRELATED DISTRELATED	TRESSES = TRESSES =	72.80 PERCENT D 27.20 PERCENT D .00 PERCENT D	DEDUCT VALUES. DEDUCT VALUES. DEDUCT VALUES.
BRANCH NAME - RUNWAY BRANCH NUMBER - ROSA SECTION NUMBER - 2				LENGTH - 700 LF N WIDTH - 50 LF N AREA - 3888 SY
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	02/89 P SAFETY- ES IN SECTION JEYED- D BE SURVEYE	CI= 88 DRAINAGE- N=	RATING≖ EXCELI SHOULDERS-	LENT OVERALL- 7 2
SAMPLE UNIT-2 (RANDON	1) S.	AMPLE SIZE-	5000 SF	SAMPLE PCI- 87
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 230 LF	DENSITY-PCT 4.60	DEDUCT-VALUE 13.1
SAMPLE UNIT-5 (RANDON				
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 215 LF	DENSITY-PCT 4.30	DEDUCT-VALUE 12.5
EXTRAPOLATED DISTRESS				
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 1558 LF	DENSITY-PGT 4.45	DEDUCT-VALUE 12.8
*** PERCENT OF DI LOAD CLIMATE/DURABILITY OTHER	RELATED DIS	TRESSES = 1	.00 PERCENT I	DEDUCT VALUES. DEDUCT VALUES.

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BRANCH NAME - RUNWAY	13-31		SECTION	LENGTH - 700 LF
BRANCH NUMBER - ROSA			SECTION	WIDTH - 50 LF
BRANCH NAME - RUNWAY BRANCH NUMBER - RO5A SECTION NUMBER - 3			SECTION	AREA - 3888 SY
INSPECTION DATE - 06/0				
CONDITION- RIDING-			SHOULDERS-	
TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV		M=		7 2
RECOMMENDED SAMPLES TO		- m		7
STANDARD DEVIATION OF			TS SURVEYED=	
SAMPLE UNIT-1 (RANDOM) S	SAMPLE SIZE-	5000 SF	SAMPLE PCI- 87
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
43 BLOCK CR	LOW	100 SF	2.00	9.9
DISTRESS TYPE 43 BLOCK CR 48 LONG/TRANS CR	LOW	230 LF	4.60	13.1
SAMPLE UNIT-6 (RANDOM				
NIGENDAG MUDD	0211222	A11.1707011	DDWGTMV DAM	D DD1100 111110
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
48 LUNG/TRANS CK	LOW	320 LF	6.40	10.8
52 WEATHER/RAVEL	HIGH	10 SF	0.20	7.9
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION	I-	
DISTRESS TYPE	CEVEDITY	OUANTTTV	nemetry der	מבמוכת עאווב
V3 BIOCK CD	SEARKTII	350 CF	1 00	7 Q
43 BLOCK CR 48 LONG/TRANS CR	LOW	1925 LF	5.50	15.0
52 WEATHER/RAVEL	HIGH	35 SF	0.10	6.0
,				
*** PERCENT OF DE				
LOAD				
CLIMATE/DURABILITY	RELATED DIS	TRESSES = 1	.00.00 PERCENT I	EDUCT VALUES.
OTHER	RELATED DIS	TRESSES =	.00 PERCENT I	DEDUCT VALUES.
BRANCH NAME - RUNWAY				LENGTH - 1760 LF
BRANCH NUMBER - ROGC			SECTION	WIDTH - 50 LF
SECTION NUMBER - 1			SECTION	N AREA - 9777 SY
			DEGITOR	
INSPECTION DATE - 06/0	2/89 F	CI= 89	RATING= EXCELI	ENT
CONDITION- RIDING-	SAFETY-	DRAINAGE-	SHOULDERS-	OVERALL-
TOTAL NUMBER OF SAMPLE	S IN JECTIO)Ņ⇔		18
NUMBER OF SAMPLES SURV				3
RECOMMENDED SAMPLES TO				6
STANDARD DEVIATION OF			TS SURVEYED=	3.2
SAMPLE UNIT-11 (RANDOM			5000 SF	SAMPLE PCI- 90
DICMPROA MUNE	William www	ATT : 13#5= =====	N MITA WALL	Danisan
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	
48 LONG/TRANS CR	LOW	TOO LF	3.00	10.0

SAMPLE UNIT-17 (RANDO	M) S.	AMPLE SIZE-	5000 SF	SAMPLE PCI- 85
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR 50 PATCHING	SEVERITY LOW LOW LOW	QUANTITY 20 SF 200 LF 10 SF	DENSITY-PCT 0.40 4.00 0.20	DEDUCT-VALUE 12.5 11.9 2.0
SAMPLE UNIT-5 (RANDO	M) S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 91
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 120 LF	DENSITY-PCT 2.40	DEDUCT-VALUE 0 8.8
EXTRAPOLATED DISTRESS				
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR 50 PATCHING	SEVERITY LOW LOW LOW	QUANTITY 117 SF 2759 LF 59 SF	DENSITY-PCT 0.13 3.13 0.06	DEDUCT-VALUE 7.5 10.2 1.2
*** PERCENT OF D LOAD CLIMATE/DURABILITY OTHER	RELATED DIS	TRESSES = 3 TRESSES = 6	9.68 PERCENT D	EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - RUNWA BRANCH NUMBER - RO6 SECTION NUMBER - 2	Y 13-31		SECTION SECTION SECTION	LENGTH - 1760 LF WIDTH - 50 LF AREA - 9777 SY
INSPECTION DATE - 06/CONDITION- RIDING-TOTAL NUMBER OF SAMPLES SURRECOMMENDED SAMPLES TANDARD DEVIATION OF	Y 13-31 C O2/89 P SAFETY- ES IN SECTION VEYED- O BE SURVEYE PCI BETWEEN	CI= 83 R DRAINAGE- N= D= RANDOM UNIT	SECTION SECTION SECTION SECTION ATING VERY G SHOULDERS-	OOD OVERALL- 18 3 5
INSPECTION DATE - 06/ CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES T	Y 13-31 C O2/89 P SAFETY- ES IN SECTION VEYED- O BE SURVEYE PCI BETWEEN	CI= 83 R DRAINAGE- N= D= RANDOM UNIT	SECTION SECTION SECTION SECTION ATING VERY G SHOULDERS-	OOD OVERALL- 18 3 5
INSPECTION DATE - 06/ CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES T STANDARD DEVIATION OF	Y 13-31 C O2/89 P SAFETY- ES IN SECTION VEYED- O BE SURVEYE PCI BETWEEN M) S. SEVERITY LOW	CI= 83 R DRAINAGE- N= C= RANDOM UNIT AAPLE SIZE- QUANTITY 340 LF	SECTION SECTION SECTION SECTION SECTION ATING VERY G SHOULDERS- SSURVEYED- 5000 SF DENSITY-PCT 6.80	OOD OVERALL- 18 3 5 1.0 SAMPLE PCI- 82 DEDUCT-VALUE
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES T STANDARD DEVIATION OF SAMPLE UNIT-1> (RANDO	Y 13-31 C O2/89 P SAFETY- ES IN SECTION VEYED- O BE SURVEYE PCI BETWEEN M) S. SEVERITY LOW	CI= 83 R DRAINAGE- N= C= RANDOM UNIT AAPLE SIZE- QUANTITY 340 LF	SECTION SECTION SECTION SECTION ATING VERY G SHOULDERS- SSURVEYED DENSITY-PCT 6.80	OOD OVERALL- 18 3 5 1.0 SAMPLE PCI- 82 DEDUCT-VALUE 17.6
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES T STANDARD DEVIATION OF SAMPLE UNIT-15 (RANDO DISTRESS TYPE 48 LONG/TRANS CR DISTRESS TYPE 48 LONG/TRANS CR	Y 13-31 C O2/89 P SAFETY- ES IN SECTION VEYED- O BE SURVEYE PCI BETWEEN M) S. SEVERITY LOW M) S.	CI= 83 R DRAINAGE- N= C= RANDOM UNIT AAPLE SIZE- QUANTITY 340 LF AMPLE SIZE- QUANTITY AMPLE SIZE-	SECTION SECTION SECTION SECTION SECTION ATING VERY G SHOULDERS- SSURVEYED- 5000 SF DENSITY-PCT 6.80 DENSITY-PCT 6.40	OOD OVERALL- 18 3 5 1.0 SAMPLE PCI- 82 DEDUCT-VALUE 17.6 SAMPLE PCI- 83 DEDUCT-VALUE 16.8
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES T STANDARD DEVIATION OF SAMPLE UNIT-15 (RANDO DISTRESS TYPE 48 LONG/TRANS CR SAMPLE UNIT-3 (RANDO DISTRESS TYPE	Y 13-31 C O2/89 P SAFETY- ES IN SECTION VEYED- O BE SURVEYE PCI BETWEEN M) S. SEVERITY LOW M) S.	CI= 83 R DRAINAGE- N= RANDOM UNIT AAPLE SIZE- QUANTITY 340 LF AMPLE SIZE- QUANTITY 320 LF	SECTION SECTION SECTION SECTION SECTION ATING— VERY G SHOULDERS- S SURVEYED— 5000 SF DENSITY-PCT 6.80 5000 SF DENSITY-PCT 6.40	OOD OVERALL- 18 3 5 1.0 SAMPLE PCI- 82 DEDUCT-VALUE 17.6 SAMPLE PCI- 83 DEDUCT-VALUE 16.8

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 5665 LF 6.43 16.8
*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES00 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES - 100.00 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES00 PERCENT DEDUCT VALUES.
BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 1760 LF BRANCH NUMBER - RO6C SECTION WIDTH - 50 LF SECTION NUMBER - 3 SECTION AREA - 9777 SY
INSPECTION DATE - 06/02/89 PCI= 84 RATING= VERY GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 18 NUMBER OF SAMPLES SURVEYED= 3 RECOMMENDED SAMPLES TO BE SURVEYED= 6 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 3.2
SAMPLE UNIT-12 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 88
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 195 LF 3.90 11.7
SAMPLE UNIT-15 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 82
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 360 LF 7.20 18.3
SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE- 500C SF SAMPLE PCI- 83
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 320 LF 6.40 16.8
EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 5136 LF 5.83 15.6
*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES00 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES - 100.00 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES00 PERCENT DEDUCT VALUES.
BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 90 LF BRANCH NUMBER - RO7C SECTION WIDTH - 50 LF SECTION NUMBER - 1 SECTION AREA - 500 SY

INSPECTION DATE - 06/02/89 PCI= 88 RATING= EXCELLENT CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 1 NUMBER OF SAMPLES SURVEYED= 1 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.
SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 88
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 200 LF 4.00 11.9
EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION- DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 180 LF 4.00 11.9
*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES.
BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 90 LF BRANCH NUMBER - RO7C SECTION WIDTH - 50 LF SECTION NUMBER - 2 SECTION AREA - 500 SY
INSPECTION DATE - 06/02/89 PCI= 84 RATING= VERY GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 1 NUMBER OF SAMPLES SURVEYED= 1 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.
SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 84
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 295 LF 5.90 15.8
EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-
DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 266 LF 5.91 15.8
*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES00 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES - 100.00 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES00 PERCENT DEDUCT VALUES.
BRANCH NAME - RUNWAY 13-31 SECTION LENGTH - 90 LF BRANCH NUMBER - RO7C SECTION WIDTH - 50 LF SECTION NUMBER - 3 SECTION AREA - 500 SY

INSPECTION DATE - 06/02/89 PCI= 79 CONDITION- RIDING- SAFETY- DRAINAGE- TOTAL NUMBER OF SAMPLES IN SECTION= NUMBER OF SAMPLES SURVEYED= RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.	SHOULDERS- OVERALL- 1 1
SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE-	5000 SF SAMPLE PCI- 79
DISTRESS TYPE SEVERITY QUANTITY 48 LONG/TRANS CR LOW 448 LF	DENSITY-PCT DEDUCT-VALUE 8.96 21.5
EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION	
DISTRESS TYPE SEVERITY QUANTITY 48 LONG/TRANS CR LOW 403 LF	DENSITY-PCT DEDUCT-VALUE 8.95 21.5
*** PERCENT OF DEDUCT VALUES BASED ON D LOAD RELATED DISTRESSES = CLIMATE/DURABILITY RELATED DISTRESSES = 1 OTHER RELATED DISTRESSES =	.00 PERCENT DEDUCT VALUES00.00 PERCENT DEDUCT VALUES00 PERCENT DEDUCT VALUES.
BRANCH NAME - RUNWAY 13-31 BRANCH NUMBER - RO8C SECTION NUMBER - 1	SECTION LENGTH - 600 LF SECTION WIDTH - 50 LF SECTION AREA - 3333 SY
INSPECTION DATE - 06/02/89 PCI= 96 CONDITION- RIDING- SAFETY- DRAINAGE- TOTAL NUMBER OF SAMPLES IN SECTION= NUMBER OF SAMPLES SURVEYED= RECOMMENDED SAMPLES TO BE SURVEYED= STANDARD DEVIATION OF PCI BETWEEN RANDOM UNI	6 2 5
SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE-	5000 SF SAMPLE PCI- 95
DISTRESS TYPE SEVERITY QUANTITY 48 LONG/TRANS CR LOW 50 LF	1.00 5.5
SAMPLE UNIT-4 (RANDOM) SAMPLE SIZE-	
DISTRESS TYPE SEVERITY QUANTITY 48 LONG/TRANS CR LOW 30 LF	DENSITY-PCT DEDUCT-VALUE 0.60 4.1
EXTRAPOLATED DISTRESS QUANTITIES FOR SECTIO	
DISTRESS TYPE SEVERITY QUANTITY 48 LONG/TRANS CR LOW 240 L	DENSITY-PCT DEDUCT-VALUE 0.80 4.7
*** PERCENT OF DEDUCT VALUES BASED ON D LOAD RELATED DISTRESSES = CLIMATE/DURABILITY RELATED DISTRESSES = 1	.00 PERCENT DEDUCT VALUES.

BRANCH NAME - RUNWAY BRANCH NUMBER - ROSC SECTION NUMBER - 2	13-31			N LENGTH - 600 LF ON WIDTH - 50 LF ON AREA - 3333 SY
INSPECTION DATE - 06/02 CONDITION- RIDING- TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	2/89 F SAFETY- S IN SECTION EYED= BE SURVEYE	PCI= 91 DRAINAGE- DN= ED= V RANDOM UNI	RATING EXCES	LLENT OVERALL- 6 2 6
SAMPLE UNIT-2 (RANDOM)			- 5000 SF	SAMPLE PCI- 89
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 185 LF	DENSITY-PC	DEDUCT-VALUE 11.3
SAMPLE UNIT-5 (RANDOM) 5	SAMPLE SIZE	- 5000 SF	SAMPLE PCI- 92
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 105 LF	DENSITY-PC	T DEDUCT-VALUE 0 8.2
EXTRAPOLATED DISTRESS OF DISTRESS TYPE 48 LONG/TRANS CR	QUANTITIES	FOR SECTION	1 -	
*** PERCENT OF DEDUCTION TO THE CONTRACT OF TH	T VALUES BARELATED DIS	ASED ON DIST STRESSES = STRESSES = S	RESS MECHANIS .00 PERCENT 100.00 PERCENT	M *** DEDUCT VALUES. DEDUCT VALUES.
*** PERCENT OF DEDUC' LOAD CLIMATE/DURABILITY	T VALUES BARELATED DISTRELATED DISTRELATED DISTRELATED DISTRELATED DISTRELATED DISTRELATED DISTRELATED DISTRELATED DISTRELATED	ASED ON DIST STRESSES = STRESSES	TRESS MECHANIS .00 PERCENT 100.00 PERCENT .00 PERCENT SECTIO SECTI	M *** DEDUCT VALUES. DEDUCT VALUES. DEDUCT VALUES. N LENGTH - 600 LF ON WIDTH - 50 LF ON AREA - 3333 SY
*** PERCENT OF DEDUCTION OF DED	T VALUES BARELATED DISTRELATED DISTRELA	ASED ON DIST STRESSES = 1 STRESSES = 1 STRESSES = 2 DRAINAGE-ON=	RESS MECHANIS .00 PERCENT .00 PERCENT .00 PERCENT SECTIO SECTI SECTI	M *** DEDUCT VALUES. DEDUCT VALUES. DEDUCT VALUES. N LENGTH - 600 LF ON WIDTH - 50 LF ON AREA - 3333 SY
*** PERCENT OF DEDUC' LOAD CLIMATE/DURABILITY OTHER BRANCH NAME - RUNWAY BRANCH NUMBER - ROSC SECTION NUMBER - 3 INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV	T VALUES BARELATED DISTRELATED DISTRELATED DISTRELATED DISTRELATED DISTRICT OF THE PROPERTY OF	ASED ON DIST STRESSES = STRESSES	RESS MECHANIS .00 PERCENT .00 PERCENT .00 PERCENT SECTIO SECTI SECTI RATING= EXCE SHOULDERS-	M *** DEDUCT VALUES. DEDUCT VALUES. DEDUCT VALUES. N LENGTH - 600 LF ON WIDTH - 50 LF ON AREA - 3333 SY LLENT OVERALL- 6 1

DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 630 LF 2.10 8.2 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = .00 PERCENT DEDUCT VALUES. BRANCH NAME - RUNWAY 13-31 SECTION LENGTH -BRANCH NUMBER - RO9C SECTION WIDTH - 50 LF SECTION NUMBER - 1 SECTION AREA - 20000 SY INSPECTION DATE - 06/02/89 PCI= 91 RATING= EXCELLENT CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVER 15 OF THE PROPERTY O OVERALL .-TOTAL NUMBER OF SAMPLES IN SECTION= NUMBER OF SAMPLES SURVEYED= 10 RECOMMENDED SAMPLES TO BE SURVEYED= STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= SAMPLE UNIT-10 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 92 SEVERITY QUANTITY DENSITY-PCT DISTRESS TYPE 48 LONG/TRANS CR LOW 110 LF 2.20 SAMPLE UNIT-15 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 92 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 100 LF 2.00 8.0 SAMPLE UNIT-17 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 91 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 125 LF 2.50 9.0 SAMPLE UNIT-20 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 92 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 48 LONG/TRANS CR LOW 105 LF 2.10 -----SAMPLE UNIT-22 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 91 SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE DISTRESS TYPE 48 LONG/TRANS CR LOW 125 LF 2.50 SAMPLE UNIT-25 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 91 DISTRESS TYPE SEVERITY QUANTITY DENSITY-PCT DEDUCT-VALUE 8 LONG/TRANS CR LOW 115 LF 2.30 8.6 48 LONG/TRANS CR

SAMPLE UNIT-31 (RANDOM) SAMPLE SIZE- 5000 SF SAMPLE PCI- 90

DISTRESS TYPE 48 LONG/TRANS CR SAMPLE UNIT-36 (RANDOM)	SEVERITY LOW	QUANTITY 140 LF	DENSITY-PCT 2.80	DEDUCT-VALUE 9.6
SAMPLE UNIT-36 (RANDOM)	S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 87
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 225 LF	DENSITY-PCT 4.50	DEDUCT-VALUE 12.9
SAMPLE UNIT-5 (RANDOM)	S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 92
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 100 LF	DENSITY-PCT 2.00	DEDUCT-VALUE 8.0
SAMPLE UNIT-7 (RANDOM)				
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 100 LF	DENSITY-PCT 2.00	DEDUCT-VALUE 8.0
EXTRAPOLATED DISTRESS Q				
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 4482 LF	DENSITY-PCT 2.49	DEDUCT-VALUE 8.9
	ELATED DIS ELATED DIS ELATED DIS	TRESSES = 10 TRESSES = 10 TRESSES =	.00 PERCENT D	DEDUCT VALUES. DEDUCT VALUES. DEDUCT VALUES.
BRANCH NAME - RUNWAY BRANCH NUMBER - R09C SECTION NUMBER - 2	13-31		SECTION SECTION SECTION	LENGTH - 3600 LF WIDTH - 50 LF WAREA - 20000 SY
INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF P	/89 P AFETY- IN SECTIO YED- BE SURVEYE	CI- 90 I DRAINAGE- N-	RATING- EXCELL SHOULDERS-	36 4 5
SAMPLE UNIT-14 (RANDOM)	S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 94
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 65 LF	DENSITY-PCT 1.30	DEDUCT-VALUE 6.1
SAMPLE UNIT-2 (RANDOM)				
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 175 LF	DENSITY-PCT 3.5	DEDUCT-VALUE 0 10.9
SAMPLE UNIT-24 (RANDOM)				

DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 145 LF	DENSITY-PCT 2.90	DEDUCT-VALUE 9.8
SAMPLE UNIT-34 (RANDOM)	S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 88
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 200 LF	DENSITY-PCT 4.00	11.9
EXTRAPOLATED DISTRESS C	QUANTITIES	FOR SECTION		
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 5265 LF	DENSITY-PCT 2.92	DEDUCT-VALUE 9.8
*** PERCENT OF DEL LOAD F CLIMATE/DURABILITY R OTHER F	ELATED DIS	TRESSES = TRESSES = 10	.00 PERCENT D	EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - RUNWAY BRANCH NUMBER - RO9C SECTION NUMBER - 3	13-31		SECTION SECTION SECTION	LENGTH - 3600 LF WIDTH - 50 LF AREA - 20000 SY
INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF F	/89 P AFETY- IN SECTION YED= BE SURVEYE	DRAINAGE- N= D=	RATING- EXCELL SHOULDERS-	ENT OVERALL- 36 4 5
SAMPLE UNIT-11 (RANDOM)	S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 96
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 25 LF	DENSITY-PCT 0.50	DEDUCT-VALUE 3.9
SAMPLE UNIT-2 (RANDOM)	S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 94
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 75 LF	DENSITY-PCT 1.50	DEDUCT-VALUE 6.5
SAMPLE UNIT-20 (RANDOM)	S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 89
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 185 LF	DENSITY-PCT 3.70	DEDUCT-VALUE 11.3
SAMPLE UNIT-28 (RANDOM)	S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 94
DISTRESS TYPE	CEUEDITV	OHANTITY	<u>ኮሮክሮፒጥ</u> V .	DEDUCT-VALUE
48 LONG/TRANS CR	LOW	70 LF	1.40	6.3

DISTRESS TYPE 38 LONG/TRANS CR	SEVERITY LOW	QUANTITY 3195 LF	DENSITY-PCT 1.77	DEDUCT-	VALUE 7.3
*** PERCENT OF DI LOAD CLIMATE/DURABILITY OTHER	RELATED DIS RELATED DIS RELATED DIS	TRESSES = 10 TRESSES = 10 TRESSES =	.00 PERCENT DE 0.00 PERCENT DE .00 PERCENT DE	DUCT VALUE DUCT VALUE DUCT VALUE	S.
BRANCH NAME - RUNWAY BRANCH NUMBER - R104 SECTION NUMBER - 1	7 13-31 A		SECTION I SECTION SECTION	LENGTH - WIDTH - AREA -	700 LF 50 LF 3888 SY
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLI NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE	02/89 P SAFETY- ES IN SECTION VEYED= UNITS TO BE	CI= 81 R DRAINAGE- N≖ SURVEYED.	ATING= VERY GO	OOD	
SAMPLE UNIT-1 (RANDO			5000 SF	SAMPLE PC	- 81
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 375 LF	DENSITY-PCT 7.50	DEDUCT-	VALUE 18.9
SAMPLE UNIT-5 (RANDO					
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	385 LF	7.70		19.2
EXTRAPOLATED DISTRESS	QUANTITIES				
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 2660 LF	DENSITY-PCT 7.60	DEDUCT	
*** PERCENT OF DELOAD CLIMATE/DURABILITY OTHER	RELATED DIS	TRESSES = 10	.00 PERCENT DE	EDUCT VALUE	ES.
BRANCH NAME - RUNWAY BRANCH NUMBER - R10 SECTION NUMBER - 2	Y 13-31 A		SECTION) SECTION SECTION	LENGTH - WIDTH - AREA -	700 LF 50 LF 3888 SY
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	02/89 P SAFETY- ES IN SECTIO VEYED- D BE SURVEYE PCI BETWEEN	CI= 83 R DRAINAGE- N= D= RANDOM UNIT	ATING= VERY GO SHOULDERS- S SURVEYED=	OOD OVERALL- 7 2 5 1.4	
SAMPLE UNIT-3 (RANDO					

DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 295 LF	DENSITY-PCT 5.90	DEDUCT-VALUE 15.8
SAMPLE UNIT-6 (RANDO	M) S.	AMPLE SIZE-	5000 SF	SAMPLE PCI- 82
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 340 LF	DENSITY-PCT 6.80	DEDUCT-VALUE 17.6
EXTRAPOLATED DISTRESS				
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 2223 LF	DENSITY-PCT 6.35	DEDUCT-VALUE 16.7
*** PERCENT OF D LOAD CLIMATE/DURABILITY OTHER	RELATED DISTRELATED DISTRELATED DISTRELATED	TRESSES = 10 TRESSES = 10 TRESSES =	.00 PERCENT D 00.00 PERCENT D .00 PERCENT D	EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - RUNWA BRANCH NUMBER - R10 SECTION NUMBER - 3	Y 13-31 A		SECTION SECTION SECTION	LENGTH - 700 LF WIDTH - 50 LF AREA - 3888 SY
INSPECTION DATE - 06/CONDITION- RIDING-TOTAL NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES TANDARD DEVIATION OF	SAFETY- ES IN SECTION VEYED= O BE SURVEYE PCI BETWEEN	DRAINAGE- N= D= RANDOM UNI	SHOULDERS-	OVERALL- 7 2 7
SAMPLE UNIT-2 (RANDO			5000 SF	SAMPLE PCI- 83
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 320 LF	DENSITY-PCT 6.40	DEDUCT-VALUE 16.8
SAMPLE UNIT-4 (RANDO	M) S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 88
DISTRESS TYPE 48 LONG/TRANS CR				
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION	-	
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 1855 LF	DENSITY-PCT 5.30	
CLIMATE/DURABILITY	RELATED DIST	TRESSES = 10	.00 PERCENT D	EDUCT VALUES. EDUCT VALUES.

BRANCH NAME - RUNWAY BRANCH NUMBER - R11A SECTION NUMBER - 1	13-31			SLAB LE SLAB WI NUMBER	ENGTH - 12.5 LF EDTH - 12.5 LF OF SLABS - 96
INSPECTION DATE - 06/02	/89	PCI= 44	RA'	TING= FAIR	
CONDITION- RIDING- S	AFETY-			SHOULDERS -	
TOTAL NUMBER OF SAMPLES		ON=			1
NUMBER OF SAMPLES SURVE		a annimin	.		2
RECOMMEND ALL SAMPLE UN	IIS TO BE	SURVEYE.	υ.		
SAMPLE UNIT-2 (RANDOM)		SAMPLE S	IZE-	20 SLABS	SAMPLE PCI- 48
DISTRESS TYPE	SEVERITY	QUANT	ITY	DENSITY-PCT	DEDUCT-VALUE
62 CORNER BR	LOW	1	SLABS	5.00	4.0
62 CORNER BR	MEDIUM	1	SLABS	5.00	8.2
62 CORNER BR 62 CORNER BR 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING	HIGH	1	SLABS	5.00	5.1
74 JOINT SPALLING	HIGH	1	SLABS	5.00	13.2
74 JOINT SPALLING	LOW	6	SLABS	30.00	7.8
74 JOINT SPALLING	MEDIUM	5	SLABS	25.00	16.0
65 JT SEAL DAMAGE	HIGH	20	SLABS	100.00	12.0
67 LG PATCH/UTIL	LOW	4	SLABS	20.00	10.0
73 SHRINKAGE CR	N/A	8	SLABS	40.00	5.9
65 JT SEAL DAMAGE 67 LG PATCH/UTIL 73 SHRINKAGE CR 66 SMALL PATCH	LOW	5	SLABS	25.00	3.2
SAMPLE UNIT-4 (RANDOM)		SAMPLE S	IZE-	20 SLABS	SAMPLE PCI- 40
DISTRESS TYPE 62 CORNER BR 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR	SEVERITY	OUANT	ΤΤΥ	DENSTTY-PCT	DEDUCT-VALUE
62 CORNER BR	HIGH	1	SLABS	5.00	12.5
74 JOINT SPALLING	HIGH	6	SLABS	30.00	34.9
74 JOINT SPALLING	LOW	3	SLABS	15.00	4.7
74 JOINT SPALLING	MEDIUM	1	SLABS	5.00	4.5
65 JT SEAL DAMAGE	HIGH	20	SLABS	100.00	12.0
63 LINEAR CR	LOW	1	SLABS	5.00	4.9
73 SHRINKAGE CR	N/A	6	SLABS	30.00	4.2
73 SHRINKAGE CR 66 SMALL PATCH	LOW	2	SLABS	10.00	1.1
EXTRAPOLATED DISTRESS Q					
DISTRESS TYPE	SEVERITY	OUANT	ITY	DENSITY-PCT	DEDUCT-VALUE
62 CORNER BR	HIGH	-	SLABS		5.2
62 CORNER BR	LOW		SLABS		1.5
62 CORNER BR	MEDIUM		SLABS		3.6
75 CORNER SPALLING	HIGH		SLABS		2.4
74 JOINT SPALLING	HIGH		SLABS		27.8
74 JOINT SPALLING	LOW		SLABS		6.4
74 JOINT SPALLING	MEDIUM		SLABS		10.6
65 JT SEAL DAMAGE	HIGH		SLABS		12.0
67 LG PATCH/UTIL	LOW		SLABS		6.1
63 LINEAR CR	LOW		SLABS		2.0
73 SHRINKAGE CR	N/A		SLABS		5.0
66 SMALL PATCH	LOW		SLABS		1.9
			I	27.70	1.7

SLAB WIDTH - 12.5 LF NUMBER OF SLABS - 96 BRANCH NUMBER - R11A SECTION NUMBER - 2 ______ INSPECTION DATE - 06/02/89 PCI= 49 RATING= FAIR CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-TOTAL NUMBER OF SAMPLES IN SECTION= NUMBER OF SAMPLES SURVEYED= RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 16 SLABS SAMPLE PCI- 57
 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 62 CORNER BR
 MEDIUM
 1 SLABS
 6.25
 10.3

 74 JOINT SPALLING
 LOW
 2 SLABS
 12.50
 4.1

 74 JOINT SPALLING
 MEDIUM
 2 SLABS
 12.50
 9.5

 65 JT SEAL DAMAGE
 HYGH
 16 SLABS
 100.00
 12.0

 57 LG PATCH/UTIL
 MEDIUM
 1 SLABS
 6.25
 12.8

 73 SHRINKAGE CR
 N/A
 12 SLABS
 75.00
 11.2

 66 SMALL PATCH
 LOW
 4 SLABS
 25.00
 3.2
 10.3 4.1 9.5 12.0 12.8 ______ SAMPLE UNIT-3 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 41
 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 62 CORNER BR
 HIGH
 1 SLABS
 5.00
 12.5

 75 CORNER SPALLING
 HIGH
 1 SLABS
 5.00
 5.1

 74 JCINT SPALLING
 HIGH
 2 SLABS
 10.00
 20.8

 74 JOINT SPALLING
 MEDIUM
 1 SLABS
 5.00
 4.7

 74 JOINT SPALLING
 MEDIUM
 1 SLABS
 5.00
 4.5

 65 JT SEAL DAMAGE
 HIGH
 20 SLABS
 100.00
 12.0

 63 LINEAR CR
 HIGH
 1 SLABS
 5.00
 16.2

 63 LINEAR CR
 LOW
 1 SLABS
 5.00
 4.9

 73 SHRINKAGE CR
 N/A
 6 SLABS
 30.00
 4.2

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

CLIMATE/DURABILITY RELATED DISTRESSES = 14.20 PERCENT DEDUCT VALUES.

OTHER RELATED DISTRESSES = 71.24 PERCENT DEDUCT VALUES.

RELATED DISTRESSES = 14.56 PERCENT DEDUCT VALUES.

SLAB LENGTH - 12.5 LF

LOAD

BRANCH NAME - RUNWAY 13-31

EX' APOLATED DISTRESS QUANTITIES FOR SECTION-

 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 62 CORNER BR
 HIGH
 3 SLABS
 3.12
 7.8

 62 CORNER BR
 MEDIUM
 3 SLABS
 3.12
 5.1

 75 CORNER SPALLING
 HIGH
 3 SLABS
 3.12
 3.4

 75 CORNER SPALLING
 LOW
 3 SLABS
 3.12
 1.1

 74 JOINT SPALLING
 HIGH
 5 SLABS
 5.20
 13.6

 74 JOINT SPALLING
 LOW
 13 SLABS
 13.54
 4.4

 74 JOINT SPALLING
 MEDIUM
 8 SLABS
 8.33
 6.9

 65 JT SEAL DAMAGE
 HIGH
 96 SLABS
 100.00
 12.0

67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH *** PERCENT OF DI LOAD CLIMATE/DURABILITY	EDUCT VALUES RELATED DIS RELATED DIS	S BASED ON DIS STRESSES = 32 STRESSES = 14	TRESS MECHANI .23 PERCENT D .17 PERCENT D	SM *** EDUCT VALUES. EDUCT VALUES.
OTHER				
BRANCH NAME - RUNWAY BRANCH NUMBER - R114 SECTION NUMBER - 3	7 13-31		SLAB LE	NGTH - 12.5 LF
INSPECTION DATE - 06/0	02/89 1	PCI= 76 RA	TING= VERY G	OOD
CONDITION- RIDING- TOTAL NUMBER OF SAMPLI	SAFETY-	DRAINAGE-	SHOULDERS -	OVERALL- 5
NUMBER OF SAMPLES SURV	ZS IN SECTION JEYED=)[4 -		2
RECOMMEND ALL SAMPLE		SURVEYED.		_
SAMPLE UNIT-3 (RANDON	4) 5	SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 83
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
75 CORN R SPALLING 65 JT SEAL DAMAGE	HIGH	20 STARS	100.00	12.0
73 SHRINKAGE CR	N/A	5 STARS	25 00	3 5
66 SMALL PATCH	LOW	1 SLABS	5.00	0.6
SAMPLE UNIT-5 (RANDO	1)	SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 69
DISTRESS TYPE 62 CORNER BR 74 JOINT SPALLING 74 JOINT SPALLING	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
62 CORNER BR	MEDIUM	1 SLAB	5.00	8.2
74 JOINT SPALLING	HIGH	1 SLABS	5.00	13.2
74 JOINT SPALLING	LOW	2 SLABS	10.00	3.5
74 JOINT SPALLING 65 JT SEAL DAMAGE	MEDIUM HIGH	1 SLABS	5.00 100.00	4.5 12.0
		ZO SLADS	100.00	14.V
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION-		
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
	MEDIUM	2 SLABS		3.6
75 CORNER SPALLING		2 SLABS		2.4
74 JOINT SPALLING	HIGH	2 SLABS		6.7
74 JOINT SPALLING 74 JOINT SPALLING	LOW MEDIUM	5 SLABS 2 SLABS		2.3 2.1
65 JT SEAL DAMAGE	HIGH	96 SLABS		12.0
73 SHRINKAGE CR	N/A	12 SLABS	12.50	1.8
66 SMALL PATCH	LOW	2 SLABS		0.3

^{***} PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD CLIMATE/DURABILITY OTHER	RELATED DIST	TRESSES = 3 TRESSES = 5	0.00 PERCENT DE	EDUCT VALUES.
BRANCH NAME - RUNWA' BRANCH NUMBER - 001 SECTION NUMBER - 1		RUN		LENGTH - 850 LF WIDTH - 150 LF AREA - 14166 SY
INSPECTION DATE - 06/ CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES T STANDARD DEVIATION OF	SAFETY- 1 ES IN SECTION VEYED=	DRAINAGE- N≕	SHOULDERS-	OVERALL- 24 4
SAMPLE UNIT-17 (RANDO				
DISTRESS TYPE 43 BLOCK CR 48 LONG/TRANS CR	SEVERITY LOW LOW	QUANTITY 1200 SF 460 LF	DENSITY-PCT 24.00 9.20	DEDUCT-VALUE 22.5 22.0
SAMPLE UNIT-2 (RANDO				
DISTRESS TYPE 43 BLOCK CR 48 LONG/TRANS CR	SEVERITY LOW LOW	QUANTITY 600 SF 210 LF	DENSITY-PCT 8.57 3.00	DEDUCT-VALUE 16.3 10.0
SAMPLE UNIT-24 (RANDO				
DISTRESS TYPE 43 BLOCK CR 48 LONG/TRANS CR	SEVERITY LOW LOW	385 LF	7.70	DEDUCT-VALUE 28.5 19.2
SAMPLE UNIT-7 (RANDO	M) S		5000 SF	SAMPLE PCI- 86
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 260 LF	DENSITY-PCT 5.20	DEDUCT-VALUE 14.4
EXTRAPOLATED DISTRESS	QUANTITIES			
DISTRESS TYPE 43 BLOCK CR 48 LONG/TRANS CR	LOW	24940 SF	19.56	20.8
*** PERCENT OF D LOAD CLIMATE/DURABILITY OTHER	RELATED DIST	TRESSES = TRESSES = 10	.00 PERCENT DI	EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - RUNWA			SECTION	

BRANCH NUMBER - 002 SECTION NUMBER - 1	C		SECTION SECTION	WIDTH - 15 AREA - 2	50 LF 2500 SY
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SUR RECOMMEND ALL SAMPLE	02/89 SAFETY- ES IN SECTI VEYED= UNITS TO BE	PCI= 84 DRAINAGE- ON=	RATING- VERY	GOOD	
SAMPLE UNIT-3 (RANDO		SAMPLE SIZE-	5625 SF	SAMPLE PCI-	- 84
DISTRESS TYPE 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY LOW MEDIUM	QUANTITY 270 LF 90 LF	DENSITY-PCT 4.80 1.60	DEDUCT-V	/ALUE 13.5 14.6
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION	1-		
DISTRESS TYPE 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY LOW MEDIUM	QUANTITY 1080 LF 360 LF	DENSITY-PCT 4.80 1.60	DEDUCT-V	ALUE 13.5 14.6
*** PERCENT OF DE LOAD CLIMATE/DURABILITY OTHER	RELATED DI RELATED DI RELATED DI	STRESSES = 1 STRESSES = 1	.00 PERCENT I .00.00 PERCENT I	DEDUCT VALUES DEDUCT VALUES DEDUCT VALUES	5.
BRANCH NAME - RUNWAY BRANCH NUMBER - 0030 SECTION NUMBER - 1	7 04-22 O'RI C	UN BLAST	SECTION	LENGTH - WIDTH - 15 WAREA - 2	150 LF 50 LF 2500 SY
SECTION NUMBER - 1 INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE	7 04-22 O'RIC D2/89 SAFETY- ES IN SECTI VEYED= JNITS TO BE	UN BLAST PCI- 80 DRAINAGE- ON- SURVEYED.	SECTION SECTION SECTION SECTION RATING= VERY OF SHOULDERS-	LENGTH - WIDTH - 15 AREA - 2 GOOD OVERALL- 1	150 LF 50 LF 2500 SY
SECTION NUMBER - 1 INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV	7 04-22 O'R C D2/89 SAFETY- ES IN SECTI VEYED= JNITS TO BE	UN BLAST PCI= 80 DRAINAGE- ON= SURVEYED.	SECTION SECTION SECTION RATING= VERY OF SHOULDERS-	LENGTH - WIDTH - 15 AREA - 2 COOD OVERALL- 1	150 LF 50 LF 2500 SY
SECTION NUMBER - 1 INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE OF SAMPLE UNIT-2 (RANDON DISTRESS TYPE 48 LONG/TRANS CR 48 LONG/TRANS CR	7 04-22 O'RIC D2/89 SAFETY- ES IN SECTI VEYED= JNITS TO BE SEVERITY HIGH LOW	UN BLAST PCI= 80 DRAINAGE- ON= SURVEYED. SAMPLE SIZE- QUANTITY 60 LF 165 LF	SECTION SECTION SECTION SECTION RATING= VERY OF SHOULDERS- 5625 SF DENSITY-PCT 1.06 2.93	LENGTH - WIDTH - 15 AREA - 2 COOD OVERALL- 1 1 SAMPLE PCI- DEDUCT-V	150 LF 50 LF 2500 SY 80 VALUE 20.5 9.8
SECTION NUMBER - 00500 SECTION NUMBER - 1 INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE OF SAMPLE UNIT-2 (RANDON DISTRESS TYPE	C 04-22 O'REC C C C C C C C C C C C C C C C C C C	UN BLAST PCI- 80 DRAINAGE- ON- SURVEYED. SAMPLE SIZE- QUANTITY 60 LF 165 LF	SECTION SECTION SECTION SECTION RATING= VERY 6 SHOULDERS- 5625 SF DENSITY-PCT 1.06 2.93	LENGTH - WIDTH - 15 AREA - 2 COOD OVERALL- 1 1 SAMPLE PCI- DEDUCT-V	150 LF 50 LF 2500 SY 80 VALUE 20.5 9.8
SECTION NUMBER - 00500 SECTION NUMBER - 1 INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE OF SAMPLE UNIT-2 (RANDON DISTRESS TYPE 48 LONG/TRANS CR 48 LONG/TRANS CR	C 04-22 O'REC 2/89 SAFETY-ES IN SECTIVEYED-JUSTS TO BE 1/9 LOW 2/9 QUANTITIES SEVERITY HIGH LOW 1/9 HIGH LOW	UN BLAST PCI= 80 DRAINAGE- ON= SURVEYED. SAMPLE SIZE- QUANTITY 60 LF 165 LF FOR SECTION QUANTITY 240 LF 660 LF	SECTION SECTION SECTION SECTION SECTION RATING= VERY OF SHOULDERS- 5625 SF DENSITY-PCT 1.06 2.93 DENSITY-PCT 1.06 2.93	LENGTH - WIDTH - 15 VAREA - 2 COOD OVERALL- 1 1 SAMPLE PCI- DEDUCT-V	150 LF 50 LF 2500 SY 80 VALUE 20.5 9.8

BRANCH NAME - RUNWA' BRANCH NUMBER - 004 SECTION NUMBER - 1	Y 04-22 OVER	RUN	SECTION SECTIO SECTIO	LENGTH - 850 LF N WIDTH - 150 LF N AREA - 14166 SY
INSPECTION DATE - 06/CONDITION- RIDING-TOTAL NUMBER OF SAMPLE SUR RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	02/89 P SAFETY- ES IN SECTIO VEYED= O BE SURVEYE	CI= 86 DRAINAGE- N=	RATING= EXCEL SHOULDERS-	LENT OVERALL- 24 4
SAMPLE UNIT-11 (RANDO	M) S	AMPLE SIZE-	5000 SF	SAMPLE PCI- 78
DISTRESS TYPE 43 BLOCK CR 48 LONG/TRANS CR	SEVERITY LOW LOW	QUANTITY 1200 SE 125 LE	DENSITY-PCT 24.0 2.5	DEDUCT-VALUE 0 22.5 0 9.0
SAMPLE UNIT-18 (RANDO				
DISTRESS TYPE 42 BLEEDING 48 LONG/TRANS CR	SEVERITY N/A LOW	QUANTITY 10 SF 20 LF	DENSITY-PCT 0.20 0.40	DEDUCT-VALUE 1.0 3.5
SAMPLE UNIT-23 (RANDO				
DISTRESS TYPE 42 BLEEDING 43 BLOCK CR 48 LONG/TRANS CR	SEVERITY N/A LOW LOW	QUANTITY 30 SF 1200 SF 75 LF	DENSITY-PCT 0.60 24.00 1.50	DEDUCT-VALUE 3.6 22.5 6.5
SAMPLE UNIT-4 (RANDO				
DISTRESS TYPE 48 LONG/TRANS CR	SEVERITY LOW	QUANTITY 110 LF	DENSITY-PCT 2.20	DEDUCT-VALUE 8.4
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION	I-	
DISTRESS TYPE 42 BLEEDING 43 BLOCK CR 48 LONG/TRANS CR	N/A LOW	255 SF 15288 SF	0.20 11.99	1.0 18.1
CLIMATE/DURABILITY	RELATED DIS	TRESSES =	.00 PERCENT	DEDUCT VALUES. DEDUCT VALUES.
BRANCH NAME - RUNWAY				LENGTH - 150 LF

BRANCH NUMBER - SECTION NUMBER -				WIDTH - 150 LF AREA - 2500 SY
INSPECTION DATE - CONDITION- RIDING TOTAL NUMBER OF S. NUMBER OF SAMPLES RECOMMEND ALL SAM	06/02/89 - SAFETY- AMPLES IN SECT SURVEYED=	PCI= 64 DRAINAGE- ION= E SURVEYED.	RATING- GOOD	OVERALL- 4 1
SAMPLE UNIT-3 (R.	ANDOM)			
43 BLOCK CR	LOW	5625 SF	100.00	DEDUCT-VALUE 35.7
EXTRAPOLATED DIST				
				DEDUCT-VALUE 35.7
LOAD	RELATED D	ISTRESSES = 1 ISTRESSES = 1 ISTRESSES =	.00.00 PERCENT D	EDUCT VALUES.
BRANCH NAME - RUBRANCH NUMBER - SECTION NUMBER -	UNWAY 13-31 OVI 006C 1	ERRUN	SECTION SECTION SECTION	LENGTH - 850 LF WIDTH - 150 LF AREA - 14166 SY
INSPECTION DATE - CONDITION- RIDING TOTAL NUMBER OF SA NUMBER OF SAMPLES RECOMMENDED SAMPL STANDARD DEVIATION	- SAFETY- AMPLES IN SECT SURVEYED= ES TO BE SURVE N OF PCI BETWE	DRAINAGE- ION- YED- EN RANDOM UNI	SHOULDERS -	OVERALL- 24 4 5 3.5
SAMPLE UNIT-12 (R.			5000 SF	
DISTRESS TYPE 43 BLOCK CR	SEVERIT LOW	Y QUANTITY 2800 S	DENSITY-PCT F 56.0	DEDUCT-VALUE 0 29.4
SAMPLE UNIT-14 (RA				
DISTRESS TYPE 43 BLOCK CR	LOW	5000 SF	100.00	35.7
SAMPLE UNIT-19 (RA				
DISTRESS TYPE 43 BLOCK CR	SEVERIT LOW	Y QUANTITY 5000 SF	DENSITY-PCT 100.00	DEDUCT-VALUE 35.7
SAMPLE UNIT-5 (RA	ANDOM)	SAMPLE SIZE-	5000 SF	SAMPLE PCI- 64

DISTRESS TYPE 43 BLOCK CR	SEVERITY LOW	QUANTITY 5000 SF	DENSITY-PCT 100.00	DEDUCT-VALUE 35.7
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION-		
DISTRESS TYPE 43 BLOCK CR	SEVERITY LOW	QUANTITY 113386 SF	DENSITY-PCT 88.93	DEDUCT-VALUE 34.2
*** PERCENT DEDUCT LOAD CLIMATE/DURABILITY OTHER	RELATED DIS	TRESSES = 10	.00 PERCENT DEI	DUCT VALUES. DUCT VALUES.
BRANCH NAME - RUNWAY BRANCH NUMBER - 0070 SECTION NUMBER - 1			SECTION V	ENGTH - 150 LF WIDTH - 150 LF AREA - 2500 SY
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLI NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE	02/89 E SAFETY- ES IN SECTION VEYED= UNITS TO BE	PCI= 58 R DRAINAGE- N= SURVEYED.	RATING= GOOD	
SAMPLE UNIT-4 (RANDON	1) S	SAMPLE SIZE-	5625 SF	SAMPLE PCI- 58
DISTRESS TYPE 43 BLOCK CR 43 BLOCK CR	LOW MEDIUM	QUANTITY 4100 SF 1200 SF	DENSITY-PCT 72.88 21.33	DEDUCT-VALUE 31.9 30.4
EXTRAPOLATED DISTRESS				
DISTRESS TYPE 43 BLOCK CR 43 BLOCK CR	SEVERITY LOW MEDIUM	QUANTITY 16400 SF 4800 SF	DENSITY-PCT 72.88 21.33	DEDUCT-VALUE 31.9 30.4
*** PERCENT OF DI LOAD CLIMATE/DURABILITY OTHER	RELATED DIS	TRESSES = 10 TRESSES = 10	.00 PERCENT DE	DUCT VALUES. DUCT VALUES.
BRANCH NAME - RUNWAY BRANCH NUMBER - 0080 SECTION NUMBER - 1	7 13-31 OVER	RUN	SECTION SECTION	ENGTH - 850 LF WIDTH - 150 LF AREA - 14166 SY
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO	02/89 F SAFETY- ES IN SECTIO /EYED=	PCI= 92 F DRAINAGE- N=	RATING- EXCELLE	NT
				- · ·

STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED=	11.8
SAMPLE UNIT-14 (RANDOM) SAMPLE SIZE- 5000 SF	
DISTRESS TYPE SEVERITY QUANTITY DENSITY-P 48 LONG/TRANS CR LOW 75 LF	CT DEDUCT-VALUE 1.50 6.5
SAMPLE UNIT-19 (RANDOM) SAMPLE SIZE- 5000 SF	SAMPLE PCI- 75
DISTRESS TYPE SEVERITY QUANTITY DENSITY-P 43 BLOCK CR LOW 1800 SF 36. 48 LONG/TRANS CR LOW 275 LF 5.	00 25.5 50 15.0
SAMPLE UNIT-2 (RANDOM) SAMPLE SIZE- 7500 SF	
NO DISTRESS	
SAMPLE UNIT-9 (RANDOM) SAMPLE SIZE- 5000 SF	
NO DISTRESS	
EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-	
DISTRESS TYPE SEVERITY QUANTITY DENSITY-P 43 BLOCK CR LOW 10206 SF 8. 48 LONG/TRANS CR LOW 1985 LF 1.	CT DEDUCT-VALUE 00 16.0 55 6.6
*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECH LOAD RELATED DISTRESSES = .00 PERCEN CLIMATE/DURABILITY RELATED DISTRESSES = 100.00 PERCEN OTHER RELATED DISTRESSES = .00 PERCEN	T DEDUCT VALUES. T DEDUCT VALUES. T DEDUCT VALUES.
INSPECTION DATE - 06/02/89 PCI= 59 RATING= GOO CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS-TOTAL NUMBER OF SAMPLES IN SECTION= NUMBER OF SAMPLES SURVEYED= RECOMMENDED SAMPLES TO BE SURVEYED= STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED=	OVERALL- 17 3 16
SAMPLE UNIT-15 (RANDOM) SAMPLE SIZE- 21 SLABS	
DISTRESS TYPE SEVERITY QUANTITY DENSITY-P 74 JOINT SPALLING LOW 1 SLABS 4. 74 JOINT SPALLING MEDIUM 1 SLABS 4. 65 JT SEAL DAMAGE MEDIUM 21 SLABS 100. 67 LG PATCH/UTIL MEDIUM 1 SLABS 4. 63 LINEAR CR MEDIUM 6 SLABS 28.	CT DEDUCT-VALUE 76 2.1 76 4.3 00 7.0 76 10.6 57 34.0

66 SMALL PATCH	LOW	3	SLABS	14.28	1.5
SAMPLE UNIT-3 (RANDOM	S) SA	AMPLE SI	ZE-	21 SLABS	SAMPLE PCI- 76
DISTRESS TYPE 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 73 SHRINKAGE CR	SEVERITY	OUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
65 JT SEAL DAMAGE	MEDIUM	21	SLABS	100.00	7.0
63 LINEAR CR	HIGH	1	SLABS	4.76	15.7
63 LINEAR CR	LOW	$\bar{1}$	SLABS	4.76	4.6
73 SHRINKAGE CR	N/A	2	SLABS	9.52	1.4
66 SMALL PATCH	LOW	ī	SLABS	4.76	0.6
SAMPLE UNIT-9 (RANDOM					
DISTRESS TYPE	SEVERITY	QUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	LOW	2	SLABS	9.52	3.5
74 JOINT SPALLING	LOW	2	SLABS	9.52	3.3
65 JT SEAL DAMAGE	HIGH	21	SLABS	100.00	12.0
67 LG PATCH/UTIL	LOW	2	SLABS	9.52	5.7
63 LINEAR CR	HIGH	1	SLABS	4.76	15.7
63 LINEAR CR	MEDIUM	4	SLABS	19.04	27.4
66 SMALL PATCH	LOW	2	SLABS	9.52	1.0
DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH 66 SMALL PATCH	MEDIUM	4	SLABS	19.04	9.1
EXTRAPOLATED DISTRESS					
DISTRESS TYPE	SEVERITY	OUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 75 JT SEAL DAMAGE 75 JT SEAL DAMAGE 76 LG PATCH/UTIL 76 LG PATCH/UTIL 76 LG PATCH/UTIL 76 LINEAR CR 76 LINEAR CR	LOW	11	SLABS	3.27	1.2
74 JOINT SPALLING	LOW	16	SLABS	4.76	2.1
74 JOINT SPALLING	MEDIUM	5	SLABS	1.48	1.5
65 JT SEAL DAMAGE	HIGH	112	SLABS	33.33	12.0
65 JT SEAL DAMAGE	MEDIUM	224	SLABS	66.66	7.0
67 LG PATCH/UTIL	LOW	11	SLABS	3.27	2.1
67 LG PATCH/UTIL	MEDIUM	5	SLABS	1.48	3.7
63 LINEAR CR	HIGH	11	SLABS	3.27	11.8
63 LINEAR CR	LOW	5	SLABS	1.48	1.4
63 LINEAR CR	MEDIUM	53	SLABS	15.77	24.7
73 SHRINKAGE CR	N/A	11	SLABS	3.27	0.9
66 SMALL PATCH		32	SLABS	9.52	1.0
66 SMALL PATCH	MEDIUM	21	SLABS	6.25	3.3
*** PERCENT OF DE	DUCT VALUES	BASED O	N DIST	ress mechani	SM ***
LOAD	RELATED DIS	TRESSES	= 52.	13 PERCENT D	EDUCT VALUES.
CLIMATE/DURABILITY					
OTHER	RELATED DIS	TRESSES	= 21.	.73 PERCENT D	EDUCT VALUES.
BRANCH NAME - TAXIWA					NGTH - 25.0 LF
BRANCH NUMBER - TO4A					DTH - 25.0 LF
SECTION NUMBER - 1					OF SLABS - 149
					OF DEADS - 149
INSPECTION DATE - 06/0 CONDITION- RIDING-	2/89 P	CI= 80	RAT	TING= VERY G	OOD

TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF F	EYED - BE SURVEYE PCI BETWEEN	D RANDOM	UNITS	SURVEYED-	7 2 5 0.7
SAMPLE UNIT-3 (RANDOM)			ZE-	21 SLABS	SAMPLE PCI- 79
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	LOW	QUANT: 3 21 2 1	SLABS	DENSITY-PCT 14.28 100.00 9.52 4.76 4.76	DEDUCT-VALUE 4.5 12.0 8.1 1.0 0.6
SAMPLE UNIT-6 (RANDOM)		AMPLE S			SAMPLE PCI- 80
DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 66 SMALL PATCH EXTRAPOLATED DISTRESS C					DEDUCT-VALUE 1.8 12.0 4.6 6.0
DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	SEVERITY LOW LOW HIGH LOW N/A LOW	QUANT: 4 11 149 11 4 36	SLABS SLABS SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 2.68 7.38 100.00 7.38 2.68 24.16	DEDUCT-VALUE 0.9 2.8 12.0 6.6 0.8 3.0
*** PERCENT OF DEL LOAD F CLIMATE/DURABILITY F	OUCT VALUES RELATED DIS RELATED DIS	BASED (TRESSES	ON DIS: - 25 - 45 - 28	TRESS MECHANI .29 PERCENT D .98 PERCENT D .74 PERCENT D	SM *** EDUCT VALUES.
BRANCH NUMBER - TO5B SECTION NUMBER - 1				SLAB WI NUMBER	DTH - 20.0 LF OF SLABS - 114
INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF E	2/89 P SAFETY- S IN SECTIO EYED= BE SURVEYE	CI= 65 DRAINAGI N=	RA'	FING= GOOD SHOULDERS -	OVERALL- 6 2 6
SAMPLE UNIT-1 (RANDOM)) S	AMPLE S	ZE-	20 SLABS	SAMPLE PCI- 69
DISTRESS TYPE	SEVERITY	QUANT	ITY	DENSITY-PCT	DEDUCT-VALUE

75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 67 LG PATCH/UTIL 63 LINEAR CR 73 SHRINKAGE CR	HIGH LOW MEDIUM LOW MEDIUM N/A	3 1 1 1 1 6	SLABS SLABS SLABS SLABS SLABS	15.00 5.00 5.00 5.00 5.00 30.00		12.6 1.9 4.5 3.1 11.6 4.2
SAMPLE UNIT-2 (RANDOM)	S	AMPLE S	IZE-	20 SLABS	SAMPLE PC	- 60
DISTRESS TYPE 74 JOINT SPALLING 63 LINEAR CR 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	HIGH HIGH MEDIUM N/A	1 1 2 2	SLABS SLABS SLABS SLABS	5.00 5.00 10.00 10.00		13.2 16.2 19.0 1.5
EXTRAPOLATED DISTRESS Q	UANTITIES	FOR SEC	CION-			
	UCT VALUES ELATED DIS' ELATED DIS' ELATED DIS	BASED (TRESSES TRESSES TRESSES	ON DIST = 50 = 49	TRESS MECHANI .98 PERCENT D .00 PERCENT D .02 PERCENT D SECTION SECTION	SM *** EDUCT VALUI EDUCT VALUI EDUCT VALUI LENGTH - WIDTH -	ES. ES. ES. 860 LF 75 LF
SECTION NUMBER - 1				SECTION	AREA -	7166 SY
INSPECTION DATE - 06/02 CONDITION- RIDING- S. TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF PO	AFETY- IN SECTION YED= BE SURVEYEN CI BETWEEN	DRAINAGI N≖ D= RANDOM	E- S	SHOULDERS - SURVEYED=	11 3 7 4.7	
SAMPLE UNIT-10 (RANDOM)						
DISTRESS TYPE 41 ALLIGATOR CR 43 BLOCK CR 48 LONG/TRANS CR 48 LONG/TRANS CR	CEVEDITV	∩!!∧ \!T'	r ጥህ	DEMCTTV_DCT	DEDITOT	

SAMPLE UNIT-2 (RANDOM)				
DISTRESS TYPE	SEVERITY	Y QUANTITY	DENSITY-PCT	DEDUCT-VALUE
43 BLOCK CR	LOW	5625 SF	100.00	35.7
SAMPLE UNIT-6 (RANDOM)				SAMPLE PCI- 57
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
41 ALLIGATOR CR	LOW	340 SF	6.04	37.9
48 LONG/TRANS CR	LOW	475 LF	8.44	20.6
48 LONG/TRANS CR	MEDIUM	55 LF	0.97	11.2
EXTRAPOLATED DISTRESS C				
DISTRESS TYPE	SEVERTTY	OUANTITY	DENSTTY-PCT	DEDUCT-VALUE
41 ALLIGATOR CR	LOW	3132 SF	4.85	35.8
43 BLOCK CR	LOW	23780 SF	36.87	25.7
48 LONG/TRANS CR	LOW	2808 LF	4.35	12.6
DISTRESS TYPE 41 ALLIGATOR CR 43 BLOCK CR 48 LONG/TRANS CR 48 LONG/TRANS CR	MEDIUM	516 LF	0.80	10.1
CLIMATE/DURABILITY R OTHER R				
BRANCH NAME - NORTH C BRANCH NUMBER - TO7C SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES	AL PAD AC /89 AFETY- IN SECTI	CESS Poi= 45 DRAINAGE-	SECTION SECTION SECTION RATING FAIR	LENGTH - 1925 LF WIDTH - 75 LF AREA - 16041 SY OVERALL- 19
BRANCH NAME - NORTH COMMERANCH NUMBER - T07C SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION - RIDING - STOTAL NUMBER OF SAMPLES SURVE	AL PAD AC /89 AFETY- IN SECTI YED=	CESS Pore 45 DRAINAGE- ON=	SECTION SECTION SECTION RATING FAIR SHOULDERS	VERALL- 1925 LF 75 LF 16041 SY OVERALL- 19 3
BRANCH NAME - NORTH C BRANCH NUMBER - T07C SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO	AL PAD AC /89 AFETY- IN SECTI YED= BE SURVEY	CESS POI 45 DRAINAGE- ON=	SECTION SECTION SECTION RATING= FAIR SHOULDERS-	VERALL- 1925 LF 75 LF 16041 SY OVERALL- 19 3 16
BRANCH NAME - NORTH C BRANCH NUMBER - TO7C SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF P	AL PAD AC /89 AFETY- IN SECTI YED= BE SURVEY CI BETWEE	CESS Pu= 45 DRAINAGE- ON= ED= N RANDOM UNI	SECTION SECTION SECTION RATING= FAIR SHOULDERS- TS SURVEYED=	LENGTH - 1925 LF WIDTH - 75 LF AREA - 16041 SY OVERALL- 19 3 16 11.5
BRANCH NAME - NORTH COMBRANCH NUMBER - T07C SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF P SAMPLE UNIT-11 (RANDOM) DISTRESS TYPE 43 BLOCK CR 43 BLOCK CR 44 LONG/TRANS CR 48 LONG/TRANS CR	AL PAD ACTOR ACT	CESS PCI= 45 DRAINAGE- ON= ED= N RANDOM UNI	SECTION SECTION SECTION SECTION SECTION TO SECTION SECTION TO SECT	DEDUCT-VALUE 18.1 17.9 20.8
BRANCH NAME - NORTH C BRANCH NUMBER - TO7C SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF P	AL PAD ACTOR	CESS PCI= 45 DRAINAGE- ON= ED= N RANDOM UNI SAMPLE SIZE- QUANTITY 600 SF 200 SF 60 LF 375 LF 170 LF 400 SF	SECTION SECTION SECTION SECTION SECTION RATING= FAIR SHOULDERS- TS SURVEYED= 5000 SF DENSITY-PCT 12.00 4.00 1.20 7.50 3.40 8.00	DEDUCT-VALUE 18.1 17.9 20.8 41.1

48 LONG/TRANS CR	LOW LOW	330	LF	6.60	17.2
48 LONG/TRANS CR	MEDIUM	190	LF	3.80	21.8
SAMPLE UNIT-4 (RAI	NDOM)	SAMPLE SI	ZE- 50	000 SF	
DISTRESS TYPE 43 BLOCK CR 48 LONG/TRANS CR 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY	QUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
43 BLOCK CR	MEDIUM	400	SF	8.00	22.1
48 LONG/TRANS CR	HIGH	40	LF	0.80	18.0
48 LONG/TRANS CR	LOW	235	LF	4.70	13.3
48 LONG/TRANS CR	MEDIUM	100	LF	2.00	16.3
53 RUTTING	MEDIUM	200	SF	4.00	34.8
EXTRAPOLATED DISTRI	ESS QUANTITIES	FOR SECT	'ION-		
DISTRESS TYPE	SEVERITY	QUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
43 BLOCK CR	LOW	13949	SF	9.66	17.0
43 BLOCK CR 48 LONG/TRANS CR	MEDIUM	5772	SF	3.99	17.8
48 LONG/TRANS CR	HIGH	1780	LF	1.23	21.9
48 LONG/TRANS CR	LOW	9043	LF	6.26	16.5
48 LONG/TRANS CR	MEDIUM	4425	LF	3.06	19.9
53 RUTTING	MEDIUM	5772	SF	3.99	34.7
OTHER BRANCH NAME - TAI BRANCH NUMBER - TAI	XIWAY E			SLAB LI	
SECTION NUMBER - 1					OF SLABS - 381
INSPECTION DATE - C CONDITION- RIDING- TOTAL NUMBER OF SAI NUMBER OF SAMPLES S RECOMMENDED SAMPLES STANDARD DEVIATION	D6/02/89 SAFETY- MPLES IN SECTI SURVEYED= S TO BE SURVEY OF PCI BETWEE	PCI= 96 DRAINAGE ON= ED= N RANDOM	RAT	FING= EXCELI SHOULDERS - SURVEYED=	LENT OVERALL- 16 5 1.7
INSPECTION DATE - (CONDITION- RIDING-TOTAL NUMBER OF SAMPLES STRECOMMENDED SAMPLES	06/02/89 SAFETY- MPLES IN SECTI SURVEYED= S TO BE SURVEY OF PCI BETWEE	PCI= 96 DRAINAGE ON= ED= N RANDOM	RAT	FING= EXCELI SHOULDERS - SURVEYED=	LENT OVERALL- 16 5 5 1.7
INSPECTION DATE - CONDITION- RIDING- TOTAL NUMBER OF SAMPLES S RECOMMENDED SAMPLES STANDARD DEVIATION SAMPLE UNIT-1 (RAM	D6/02/89 SAFETY- MPLES IN SECTI SURVEYED= S TO BE SURVEY OF PCI BETWEE NDOM) SEVERITY	PCI= 96 DRAINAGE ON= ED= N RANDOM SAMPLE SI QUANTI	RATURITS	FING= EXCELI SHOULDERS - SURVEYED= 20 SLABS DENSITY-PCT	LENT OVERALL- 16 5 1.7 SAMPLE PCI- 98 DEDUCT-VALUE
INSPECTION DATE - CONDITION- RIDING- TOTAL NUMBER OF SAMPLES S RECOMMENDED SAMPLES STANDARD DEVIATION SAMPLE UNIT-1 (RAM	D6/02/89 SAFETY- MPLES IN SECTI SURVEYED= S TO BE SURVEY OF PCI BETWEE NDOM) SEVERITY LOW	PCI= 96 DRAINAGE ON= ED= N RANDOM SAMPLE SI QUANTI 20	RATUNITS ZE-	FING= EXCELI SHOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00	LENT OVERALL- 16 5 5 1.7 SAMPLE PCI- 98 DEDUCT-VALUE 2.0
INSPECTION DATE - CONDITION- RIDING- TOTAL NUMBER OF SAINUMBER OF SAMPLES STANDARD DEVIATION SAMPLE UNIT-1 (RAINUMBERS TYPE 65 JT SEAL DAMAGE	SAFETY- MPLES IN SECTI SURVEYED= S TO BE SURVEY OF PCI BETWEE NDOM) SEVERITY LOW NDOM) SEVERITY	PCI= 96 DRAINAGE ON= ED= N RANDOM SAMPLE SI QUANTI 20 SAMPLE SI	RATE S UNITS ZE- TY SLABS ZE-	FING= EXCELI SHOULDERS- SURVEYED= 20 SLABS DENSITY-PCT 100.00 20 SLABS DENSITY-PCT	LENT OVERALL- 16 5 5 1.7 SAMPLE PCI- 98 DEDUCT-VALUE 2.0 SAMPLE PCI- 98 DEDUCT-VALUE

DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING	SEVERITY LOW LOW	QUANTITY 1 SLABS 1 SLABS	DENSITY-PCT 5.00 5.00	DEDUCT-VALUE 1.9 2.2
SAMPLE UNIT-4 (RANDOM)	S	AMPLE SIZE-	20 SLABS	SAMPLE PCI- 95
DISTRESS TYPE 66 SMALL PATCH	SEVERITY LOW	QUANTITY 7 SLABS	DENSITY-PCT 35.00	DEDUCT-VALUE 5.0
SAMPLE UNIT-5 (RANDOM)		AMPLE SIZE-	20 SLABS	SAMPLE PCI- 94
DISTRESS TYPE 63 LINEAR CR 73 SHRINKAGE CR				T DEDUCT-VALUE 4.9 1.5
EXTRAPOLATED DISTRESS (
DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH *** PERCENT OF DEL LOAD CLIMATE/DURABILITY FOTHER BRANCH NAME - TAXIWAY BRANCH NUMBER - TO9A SECTION NUMBER - 1	LOW LOW LOW N/A LOW OUCT VALUES ELATED DIS ELATED DIS	4 SLABS 8 SLABS 76 SLABS 4 SLABS 8 SLABS 27 SLABS BASED ON DISTRESSES = 15 TRESSES = 31 TRESSES = 52 END	1.04 2.09 19.94 1.04 2.09 7.08 IRESS MECHANI .87 PERCENT D .75 PERCENT D .38 PERCENT D	0.3 1.3 2.0 1.0 0.8 0.9 SM *** EDUCT VALUES. EDUCT VALUES. EDUCT VALUES. EDUCT VALUES.
INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF E	2/89 P SAFETY- S IN SECTIO CYED= BE SURVEYE PCI BETWEEN	CI= 82 RA' DRAINAGE- S N= D= RANDOM UNITS	FING= VERY G SHOULDERS - SURVEYED=	OVERALL- 6 2 6 14.1
SAMPLE UNIT-3 (RANDOM)				
DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	SEVERITY MEDIUM MEDIUM LOW N/A LOW	QUANTITY 20 SLABS 3 SLABS 2 SLABS 2 SLABS 1 SLABS	DENSITY-PCT 100.00 15.00 10.00 10.00 5.00	DEDUCT-VALUE 7.0 20.3 8.5 1.5 0.6
SAMPLE UNIT-5 (RANDOM)	S	AMPLE SIZE-	24 SLABS	SAMPLE PCI- 92

DISTRESS TYPE 65 JT SEAL DAMAGE 73 SHRINKAGE CR 66 SMALL PATCH	LOW	I SLABS	4.16	0.5
EXTRAPOLATED DISTRESS				
DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH				
*** PERCENT OF D LOAD CLIMATE/DURABILITY OTHER	RELATED DIST RELATED DIST	TRESSES = 16 TRESSES = 26	.92 PERCENT D .32 PERCENT D .77 PERCENT D	EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - TAXIW. BRANCH NUMBER - T10.				NGTH - 25.0 LF DTH - 25.0 LF OF SLABS - 63
SECTION NUMBER - 1			NUMBER	OF SLABS - 63
SECTION NUMBER - 1 INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURTECOMMEND ALL SAMPLE	02/89 PC SAFETY- I ES IN SECTION VEYED=	CI= 53 RA' DRAINAGE-	TING= FAIR SHOULDERS-	
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLENUMBER OF SAMPLES SUR	02/89 PO SAFETY- I ES IN SECTION VEYED= UNITS TO BE S	CI= 53 RA' DRAINAGE- N= SURVEYED.	TING= FAIR SHOULDERS-	OVERALL- 5 1
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLES SURTECOMMEND ALL SAMPLE TO SAMPLE UNIT-4 (RANDO) DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR	02/89 PO SAFETY- I ES IN SECTION VEYED= UNITS TO BE S M) SA SEVERITY MEDIUM LOW LOW MEDIUM	CI= 53 RADRAINAGE-N= SURVEYED. AMPLE SIZE- QUANTITY 12 SLABS 4 SLABS 2 SLABS 3 SLABS 2 SLABS	TING= FAIR SHOULDERS- 12 SLABS DENSITY-PCT 100.00 33.33 16.66 25.00 16.66	OVERALL- 5 1 SAMPLE PCI- 53 DEDUCT-VALUE 7.0 14.2 12.1 31.8 1.8
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLES SURTECOMMEND ALL SAMPLE TO SAMPLE UNIT-4 (RANDO) DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR	02/89 PO SAFETY- I ES IN SECTION VEYED= UNITS TO BE S M) SA SEVERITY MEDIUM LOW LOW MEDIUM LOW LOW LOW LOW LOW	CI= 53 RADRAINAGE-N= SURVEYED. AMPLE SIZE- QUANTITY 12 SLABS 4 SLABS 2 SLABS 3 SLABS 2 SLABS	TING= FAIR SHOULDERS- 12 SLABS DENSITY-PCT 100.00 33.33 16.66 25.00	OVERALL- 5 1 SAMPLE PCI- 53 DEDUCT-VALUE 7.0 14.2 12.1 31.8 1.8

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 65.78 PERCENT DEDUCT VALUES.

CLIMATE/DURABILITY RELATED DISTRESSES = 10.37 PERCENT DEDUCT VALUES.

BRANCH NAME - TAXIWAY	D				NGTH - 25.0 LF
BRANCH NUMBER - T11A	-			SLAB WI	DTH - 25 0 LF
SECTION NUMBER - 1.				NUMBER	OF SLABS - 320
BRANCH NAME - TAXIWAY BRANCH NUMBER - T11A SECTION NUMBER - 1					
INSPECTION DATE - 06/02	/89	PCT== 78	PΔ°	TING UFPV C	(OO)
CONDITION DIDING C	₹₽₽₽₽	DDATMAC	C .	סמטנוז טבסכ	
TOTAL NUMBER OF SAMPLES	IN SECT	ION=			15
TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO	YED=				OVERALL- 15 3 8
RECOMMENDED SAMPLES TO	BE SURVE	YED=			8
STANDARD DEVIATION OF P	CI BETWE	EN RANDOM	UNITS	SURVEYED=	4.5
	~				
SAMPLE UNIT-13 (RANDOM)		SAMPLE S	IZE-	21 SLABS	SAMPLE PCI- 82
DISTRESS TYPE	SEVERIT	Y OUANT	ITY	DENSITY-PCT	DEDUCT-VALUE
65 JT SEAL DAMAGE	MEDIUM	21	SLABS	100.00	7.0
63 LINEAR CR	LOW	2	SLABS	9.52	8.1
73 SHRINKAGE CR	N/A	7	SLABS	33.33	4.7
66 SMALL PATCH	LOW	4	SLABS	19.04	2.1
65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH					
SAMPLE UNIT-4 (RANDOM)		SAMPLE S	IZE-	21 SLABS	SAMPLE PCI- 73
DISTRESS TYPE	SEVERITY	Y OUANT	ITY	DENSITY-PCT	DEDUCT-VALUE
65 JT SEAL DAMAGE	HIGH	` 21	SLABS	100.00	12.0
63 LINEAR CR	LOW	8	SLABS	38.09	18.6
73 SHRINKAGE CR	N/A	2	SLABS	9.52	1.4
DISTRESS TYPE 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	LOW	3	SLABS	14.28	1.5
SAMPLE UNIT-9 (RANDOM)					
DISTRESS TYPE	SEVERITY	Y QUANT	ITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING	LOW	1	SLABS	4.76	2.1
65 JT SEAL DAMAGE	MEDIUM	21	SLABS	100.00	7.0
63 LINEAR CR	LOW	5	SLABS	23.80	15.0
74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	N/A	2	SLABS	9.52	1.4
oo SMALL PATCH	LOW	2	SLABS	9.52	1.0
EXTRAPOLATED DISTRESS Q	UANTITIES	FOR SEC	CION-		
DISTRESS TYPE	SEVERITY	Y OHANT	ſΤΥ	DENSITY-PCT	DEDUCT-VALUE
	LOW	•	SLABS		
	HIGH			33.43	12.0
65 JT SEAL DAMAGE				66.56	7.0
	LOW			23.75	15.0
73 SHRINKAGE CR	N/A			17.50	2.4
66 SMALL PATCH	LOW		SLABS		1.5

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES - 38.56 PERCENT DEDUCT VALUES.

CLIMATE/DURABILITY RELATED DISTRESSES - 48.84 PERCENT DEDUCT VALUES.

BRANCH NAME - TAXIWA BRANCH NUMBER - T12B SECTION NUMBER - 1	Y C		SECTION SECTION SECTION	LENGTH - LF N WIDTH - LF N AREA - 9439 SY
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE U	S IN SECTION EYED=	PCI= 59 DRAINAGE- DN=		
SAMPLE UNIT-6 (RANDOM	()	SAMPLE SIZE	- 5000 SF	SAMPLE PCI- 59
DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR	MEDIUM	650 LF	13.00	40.8 15.0
SAMPLE UNIT-9 (RANDOM)	SAMPLE SIZE		
DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR	MEDIUM LOW	650 LF 255 LF	13.00 5.10	40.8 14.2
EXTRAPOLATED DISTRESS				
DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR	SEVERITY MEDIUM LOW	QUANTITY 11050 LF 4505 LF	DENSITY-PCT 13.00 5.30	DEDUCT-VALUE 40.8 14.6
*** PERCENT OF DE LOAD CLIMATE/DURABILITY OTHER	RELATED DIS	STRESSES = :	.00 FERCENT 1	DEDUCT VALUES. DEDUCT VALUES.
BRANCH NAME - TAXIWA BRANCH NUMBER - T13B SECTION NUMBER - 1			SLAB W	IDTH - 25.0 LF
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE U	2/89 SAFETY- S IN SECTION EYED- NITS TO BE	PCI= 85 DRAINAGE- DN= SURVEYED.	RATING VERY (SHOULDERS-	GOOD OVERALL- 2 2
SAMPLE UNIT-1 (RANDOM				
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE	SEVERITY LOW MEDIUM	QUANTITY 1 SL 21 SL	DENSITY-PCT ABS 4.76 ABS 100.00	DEDUCT-VALUE 2.1 7.0

63 LINEAR CR 70 SCALING/CRAZING	LOW LOW	4 1	SLABS SLABS	19.04 4.76	13.2 2.0
SAMPLE UNIT-2 (RANDOM	()	SAMPLE SI	ZE-	17 SLABS	SAMPLE PCI- 89
DISTRESS TYPE 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 73 SHRINKAGE CR	N/A	2	STARR	11./6	1./
EXTRAPOLATED DISTRESS					
DISTRESS TYPE 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 65 JT SEAL DAMAGE 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR *** PERCENT OF DE LOAD CLIMATE/DURABILITY OTHER	LOW MEDIUM LOW LOW LOW N/A DUCT VALUE RELATED DI RELATED DI	2 1 15 19 4 1 2 SS BASED (CSTRESSES	SLABS SLABS SLABS SLABS SLABS SLABS ON DIS' 36 34	5.88 2.94 44.11 55.88 11.76 2.94 5.88 PRESS MECHANI .54 PERCENT I	ISM *** DEDUCT VALUES. DEDUCT VALUES.
BRANCH NAME - TAXIWA BRANCH NUMBER - T14E SECTION NUMBER - 1					LENGTH - LF W WIDTH - LF N AREA - 28782 SY
BRANCH NAME - TAXIWA BRANCH NUMBER - T14E SECTION NUMBER - 1 INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE U	Y C 2/89 SAFETY- S IN SECTI	PCI- 59 DRAINAGE	RA' E- (SECTION SECTION SECTION	LENGTH - LF W WIDTH - LF W AREA - 28782 SY
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV	Y C 2/89 SAFETY- S IN SECTI EYED=	PCI= 59 DRAINAGE ON= SURVEYE	RA'.	SECTION SECTION SECTION SECTION FING COOD SHOULDERS	LENGTH - LF N WIDTH - LF N AREA - 28782 SY OVERALL- 70 7
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE U	Y C 2/89 SAFETY- S IN SECTI EYED= INITS TO BE SEVERITY MEDIUM LOW	PCI 59 DRAINAGE ON- SURVEYER SAMPLE ST 4 QUANTT 650 150	RATE- S	SECTION SECTION SECTION SECTION FING GOOD SHOULDERS-	LENGTH - LF N WIDTH - LF N AREA - 28782 SY OVERALL- 70 7 SAMPLE PCI- 59 DEDUCT-VALUE
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE U SAMPLE UNIT-16 (RANDOM DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR SAMPLE UNIT-26 (RANDOM	Y C 2/89 SAFETY- S IN SECTIVEYED- INITS TO BE SEVERITY MEDIUM LOW	PCI= 59 DRAINAGE ON= C SURVEYER SAMPLE ST 650 150 SAMPLE ST	RAY	SECTION SECTION SECTION SECTION SECTION FING* GOOD SHOULDERS- DENSITY-PCT 13.00 3.00 000 SF	LENGTH - LF N WIDTH - LF N AREA - 28782 SY OVERALL- 70 7 SAMPLE PCI- 59 DEDUCT-VALUE 40.8 10.0 SAMPLE PCI- 59
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMEND ALL SAMPLE U SAMPLE UNIT-16 (RANDOM DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR	Y C 2/89 SAFETY- S IN SECTIVEYED- INITS TO BE SEVERITY MEDIUM LOW	PCI= 59 DRAINAGE ON= C SURVEYER SAMPLE ST 650 150 SAMPLE ST	RAY	SECTION SECTION SECTION SECTION SECTION FING* GOOD SHOULDERS- DENSITY-PCT 13.00 3.00 000 SF	LENGTH - LF N WIDTH - LF N AREA - 28782 SY OVERALL- 70 7 SAMPLE PCI- 59 DEDUCT-VALUE 40.8 10.0 SAMPLE PCI- 59

DISTRESS TYPE 42 BLEEDING 47 JT REFLECT CR 48 LONG/TRANS CR 49 OIL SPILLAGE	SEVERITY N/A MEDIUM LOW N/A	QUANTI 20 650 110 5	TY SF LF LF SF	DENSITY- 0 13 2 0	PCT .40 .00 .20 .10	DEDUCT-VALUE 2.5 40.8 8.4 2.0
SAMPLE UNIT-46 (RANDOM)	5	SAMPLE SI	ZE- 5	000 SF	SA	MPLE PCI- 59
DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR	MEDIUM LOW	QUANTI 650 71	TY LF LF	DENSITY- 13 1	PCT .00 .42	DEDUCT-VALUE 40.8 6.3
SAMPLE UNIT-56 (RANDOM)		SAMPLE SI	ZE- 5	000 SF	SA	MPLE PCI- 59
DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR	SEVERITY MEDIUM LOW	QUANTI 650 188	TY LF LF	DENSITY- 13 3	PCT .00 .76	DEDUCT-VALUE 40.8 11.4
SAMPLE UNIT-6 (RANDOM)	5	SAMPLE SI	ZE- 5	000 SF	SA	MPLE PCI- 59
DISTRESS TYPE 42 BLEEDING 47 JT REFLECT CR 48 LONG/TRANS CR	N/A MEDIUM	35 650	SF LF	0 13	.70 .00	4.2 40.8
SAMPLE UNIT-66 (RANDOM)		SAMPLE SI	ZE- 5	000 SF	SA	MPLE PCI- 59
DISTRESS TYPE 42 BLEEDING 47 JT REFLECT CR 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY N/A MEDIUM LOW MEDIUM	QUANTI 120 650 110 8	SF LF LF LF	DENSITY- 2 13 2 0	PGT .40 .00 .20 .16	DEDUCT-VALUE 12.6 40.8 8.4 4.6
EXTRAPOLATED DISTRESS Q	UANTITIES	FOR SECT	NOI-			
DISTRESS TYPE 42 BLEEDING 47 JT REFLECT CR 48 LONG/TRANS CR 49 OIL SPILLAGE	LOW	1295 33670 6801	SF LF LF	0 12 2	.49 .99 .62	DEDUCT-VALUE 2.9 40.7 9.2 0.2
CLIMATE/DURABILITY R	ELATED DIS	STRESSES STRESSES	- 94	.00 PERCE	NT DEDU NT DEDU	CT VALUES.
BRANCH NAME - TAXIWAY BRANCH NUMBER - T15C SECTION NUMBER - 1		ET		SECT	TION LENTION WITION AR	NGTH - LF DTH - LF EA - 1313 SY
INSPECTION DATE - 06/02						

CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMEND ALL SAMPLE	ES IN SECTION VEYED= UNITS TO BE :	N= SURVEYED.		3 1
SAMPLE UNIT-3 (RANDO			5000 SF	
DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR	SEVERITY MEDIUM MEDIUM	QUANTITY 707 LF 24 LF	DENSITY-PCT 14.14 0.48	DEDUCT-VALUE 42.0 7.8
EXTRAPOLATED DISTRESS				
DISTRESS TYPE 47 JT REFLECT CR 48 LONG/TRANS CR	SEVERITY MEDIUM MEDIUM	QUANTITY 1669 LF 57 LF	DENSITY-PCT 14.12 0.48	DEDUCT-VALUE 42.0 7.8
*** PERCENT OF D LOAD CLIMATE/DURABILITY OTHER	RELATED DIST RELATED DIST	TRESSES = 10 TRESSES = 10 TRESSES =	.00 PERCENT DO 00.00 PERCENT DO .00 PERCENT DO	EDUCT VALUES. EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - TAXIW BRANCH NUMBER - T16 SECTION NUMBER - 1	AY C - PAD AC C	CESS		LENGTH - 280 LF WIDTH - 75 LF AREA - 2333 SY
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMEND ALL SAMPLES	02/89 PC SAFETY- I ES IN SECTION VEYED= UNITS TO BE S	CI= 77 R DRAINAGE- N= SURVEYED.	ATING= VERY GO SHOULDERS-	OOD OVERALL- 3 1
SAMPLE UNIT-2 (RANDO			5625 SF	SAMPLE PCI- 77
DISTRESS TYPE 48 LONG/TRANS CR 48 LONG/TRANS CR	LOW	400 LF 165 LF	7.11 2.93	18.2
EXTRAPOLATED DISTRESS	QUANTITIES F			
DISTRESS TYPE 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY LOW MEDIUM	QUANTITY 1492 LF 615 LF	DENSITY-PCT 7.10 2.92	DEDUCT-VALUE 18.1 19.5
CLIMATE/DURABILITY OTHER	RELATED DIST RELATED DIST RELATED DIST	RESSES = 10 RESSES = 10	.00 PERCENT DI 0.00 PERCENT DI .00 PERCENT DI	EDUCT VALUES. EDUCT VALUES.

SECTION LENGTH - 2140 LF

BRANCH NAME - TAXIWAY C

BRANCH NUMBER - T17B SECTION NUMBER - 1			SECTION SECTION	WIDTH - 75 LF AREA - 17833 SY
INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF I	2/89 P SAFETY- S IN SECTIO EYED= BE SURVEYE	CI= 69 DRAINAGE- N=	RATING= GOOD SHOULDERS-	OVERALL- 28 4 5
SAMPLE UNIT-12 (RANDOM)				
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY LOW LOW MEDIUM	QUANTITY 150 SF 115 LF 110 LF	DENSITY-PCT 2.66 2.04 1.95	DEDUCT-VALUE 30.1 8.0 16.0
SAMPLE UNIT-19 (RANDOM)				
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY LOW LOW MEDIUM	QUANTITY 260 SF 10 LF 115 LF	DENSITY-PCT 4.62 0.17 2.04	DEDUCT-VALUE 35.3 2.8 16.4
SAMPLE UNIT-24 (RANDOM)				
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR	SEVERITY LOW LOW	QUANTITY 139 SF 305 LF	DENSITY-PCT 2.47 5.42	DEDUCT-VALUE 29.3 14.8
SAMPLE UNIT-5 (RANDOM)) S	AMPLE SIZE	- 5625 SF	SAMPLE PCI- 71
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR	LOW	137 SF 230 LF	2.43 4.08	29.1 12.0
EXTRAPOLATED DISTRESS (
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY LOW LOW MEDIUM	QUANTITY 4891 SF 4706 LF 1604 LF	DENSITY-PCT 3.04 2.93 0.99	DEDUCT-VALUE 31.6 9.8 11.4
*** PERCENT OF DEI LOAD F CLIMATE/DURABILITY F OTHER F	RELATED DIS RELATED DIS RELATED DIS	TRESSES = TRESSES =	59.85 PERCENT D 40.15 PERCENT D .00 PERCENT D	DEDUCT VALUES. DEDUCT VALUES. DEDUCT VALUES.
BRANCH NAME - TAXIWAY BRANCH NUMBER - T18B SECTION NUMBER - 1	C		SECTION	LENGTH - LF WIDTH - 943 LF WAREA - 75 SY

INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	ES IN SECTIO /EYED=) BE SURVEYE	:D=			12 3 5		
STANDARD DEVIATION OF	TOI DEIWEEN	RANDON C	MIIO SOK	^ ~	J. Z		
SAMPLE UNIT-10 (RANDOM) SAMPLE SIZE- 4125 SF SAMPLE PCI- 75							
DISTRESS TYPE	SEVERITY	OUANTIT	Y DEN	SITY-PCT	DEDUCT-VALUE		
41 ALLIGATOR CR	LOW	64 S	F	1.55	24.7		
DISTRESS TYPE 41 ALLIGATOR CR 48 LONG/TRANS CR	LOW	170 I	F	4.12	12.1		
SAMPLE UNIT-3 (RANDON	í) S	AMPLE SIZ	E- 5625	SF	SAMPLE PCI- 81		
DISTRESS TYPE 43 BLOCK CR 43 BLOCK CR 48 LONG/TRANS CR 48 LONG/TRANS CR	CEVEDITTY	ለ፤፤ ላ እነ ጥፕ ባ	יאי די ער	CTTV DCT	DEDUCT VALUE		
N3 BIOCA CD	SEVERILL	150 0	.i Den	2 66	DEDUCI-VALUE		
43 BLOCK CR	MEDITIM	75 0	יבי ידי	2.00	10.7		
48 I ONG /TRANS CR	IUI	190 T	r.	3 37	10.7		
48 LONG/TRANS CR	MEDTUM	55 I	.F	0.97	11.2		
SAMPLE UNIT-7 (RANDON	í) S	AMPLE SIZ	E- 4125	SF	SAMPLE PCI- 76		
DISTRESS TYPE	SEVERITY	QUANTIT	Y DEN	SITY-PCT	DEDUCT-VALUE		
43 BLOCK CR 43 BLOCK CR	LOW	` 48 S	F	1.16	8.2		
43 BLOCK CR	MEDIUM	150 S	F	3.63	17.1		
48 LONG/TRANS CR	LOW	318 I	F	7.70	19.2		
EXTRAPOLATED DISTRESS							
	•						
DISTRESS TYPE 41 ALLIGATOR CR 43 BLOCK CR 43 BLOCK CR 48 LONG/TRANS CR 48 LONG/TRANS CR	SEVERITY	QUANTII	Y DEN	SITY-PCT	DEDUCT-VALUE		
41 ALLIGATOR CR	LOW	3 S	F	0.44	13.2		
43 BLOCK CR	LOW	10 S	F	1.48	8.9		
43 BLOCK CR	MEDIUM	11 S	F	1.62	13.6		
48 LONG/TRANS CR	LOW	34 I	JF .	5.03	14.0		
48 LONG/TRANS CR	MEDIUM	3 I	F	0.44	7.4		
*** PERCENT OF DE							
LOAD							
CLIMATE/DURABILITY							
OTHER	RELATED DIS	TRESSES -	.00	PERCENT D	EDUCT VALUES.		
BRANCH NAME - TAXIWA							
BRANCH NUMBER - T19A	I K AI DOOI	II OAL		SLAD LE	DTH - 20.0 LF		
				NIMBER	OF SLARS - 57		
					U.A.L.U - J/		
INSPECTION DATE - 06/0 CONDITION- RIDING-)2/89 P	CI= 62	RATING	- GOOD			
CONDITION- RIDING-	SAFETY-	DRAINAGE-	SHOU	LDERS-	OVERALL-		
TOTAL NUMBER OF SAMPLE	ES IN SECTIO	N=			3		
NUMBER OF SAMPLES SURV	EYED=				1		

RECOMMEND ALL SA	MPLE UNITS	TO RE	SURVEYED.
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SAMPLE UNIT-3 (RANDOM						
DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 66 SMALL PATCH	SEVERTTY	OHANTTTY	DENSITY-PCT	DEDUCT-VALUE		
65 IT SEAL DAMAGE	MEDILIM	18 STARS	100 00	7 N		
67 IG PATCH/IITII.	LOW	2 SLABS	11 11	6.4		
67 LG PATCH/UTTI.	MEDTUM	2 SLABS	11 11	17 6		
63 LINEAR CR	MEDTUM	2 SLABS	11.11	20.1		
66 SMALL PATCH	LOW	5 SLABS	27.77	3.6		
			_,,,,			
EXTRAPOLATED DISTRESS	•					
DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 66 SMALL PATCH	SEVERITY	OUANTITY	DENSITY-PCT	DEDUCT-VALUE		
65 JT SEAL DAMAGE	MEDIUM	57 SLABS	100.00	7.0		
67 LG PATCH/UTIL	LOW	6 SLABS	10.52	6.2		
67 LG PATCH/UTIL	MEDIUM	6 SLABS	10.52	17.1		
63 LINEAR CR	MEDIUM	6 SLABS	10.52	19.5		
66 SMALL PATCH	LOW	16 SLABS	28.07	3.6		
*** PERCENT OF DE LOAD CLIMATE/DURABILITY OTHER	RELATED DIS RELATED DIS RELATED DIS	TRESSES = 36 TRESSES = 13 TRESSES = 50	.52 PERCENT D .11 PERCENT D .37 PERCENT D	DEDUCT VALUES. DEDUCT VALUES. DEDUCT VALUES.		
BRANCH NAME - TAXIWAY R BRANCH NUMBER - T20A SECTION NUMBER - 1 SECTION NUMBER - 1 SLAB WIDTH - 25.0 LF NUMBER OF SLABS - 432						
SECTION NUMBER - 1			SLAB WI NUMBER	OTH - 25.0 LF OF SLABS - 432		
INSPECTION DATE - 06/0	2/89 I	CI= 64 RA	TING- GOOD			
INSPECTION DATE - 06/0	2/89 I	PCI= 64 RA	TING- GOOD	OVERALL.		
INSPECTION DATE - 06/0	2/89 I	PCI= 64 RA	TING- GOOD	OVERALL.		
INSPECTION DATE - 06/0	2/89 I	PCI= 64 RA	TING- GOOD	OVERALL.		
INSPECTION DATE - 06/0	2/89 I	PCI= 64 RA	TING- GOOD	OVERALL.		
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	PCI- 64 RA DRAINAGE- ON- CD- I RANDOM UNITS	TING- GOOD SHOULDERS- SURVEYED-	OVERALL- 21 4 18 15.0		
INSPECTION DATE - 06/0	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	CCI- 64 RA DRAINAGE- ON- CD- R RANDOM UNITS	TING- GOOD SHOULDERS- SURVEYED-	OVERALL- 21 4 18 15.0		
INSPECTION DATE - 06/0 CONDITION- RIDING-TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	CCI 64 RA DRAINAGE- ON CD CO	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49		
INSPECTION DATE - 06/0 CONDITION- RIDING-TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	CCI 64 RA DRAINAGE- ON CD CO	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49		
INSPECTION DATE - 06/0 CONDITION- RIDING-TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	CCI 64 RA DRAINAGE- ON CD CO	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49		
INSPECTION DATE - 06/0 CONDITION- RIDING-TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	CCI 64 RA DRAINAGE- ON CD CO	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49		
INSPECTION DATE - 06/0 CONDITION- RIDING-TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	CCI 64 RA DRAINAGE- ON CD CO	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49		
INSPECTION DATE - 06/0 CONDITION- RIDING-TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	CCI= 64 RA DRAINAGE- ON= I RANDOM UNITS GAMPLE SIZE-	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49		
INSPECTION DATE - 06/0 CONDITION- RIDING-TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN	CCI= 64 RA DRAINAGE- ON= I RANDOM UNITS GAMPLE SIZE-	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49		
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH	2/89 SAFETY- S IN SECTION EYED= BE SURVEYN PCI BETWEEN) SEVERITY HIGH LOW MEDIUM LOW MEDIUM LOW MEDIUM LOW	CCI= 64 RADRAINAGE- DRAINAGE- DN= I RANDOM UNITS AMPLE SIZE- QUANTITY 21 SLABS 7 SLABS 1 SLABS 4 SLABS 3 SLABS 3 SLABS	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS DENSITY-PCT 100.00 33.33 4.76 19.04 14.28 14.28	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49 DEDUCT-VALUE 12.0 14.2 10.6 13.2 23.2 1.5		
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH SAMPLE UNIT-19 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYE PCI BETWEEN	CCI= 64 RADRAINAGE- DRAINAGE- DRAINA	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS DENSITY-PCT 100.00 33.33 4.76 19.04 14.28 14.28	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49 DEDUCT-VALUE 12.0 14.2 10.6 13.2 23.2 1.5 SAMPLE PCI- 80		
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH SAMPLE UNIT-19 (RANDOM	2/89 SAFETY- S IN SECTION EYED= BE SURVEYE PCI BETWEEN	CCI= 64 RADRAINAGE- DRAINAGE- DRAINA	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS DENSITY-PCT 100.00 33.33 4.76 19.04 14.28 14.28	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49 DEDUCT-VALUE 12.0 14.2 10.6 13.2 23.2 1.5 SAMPLE PCI- 80		
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH SAMPLE UNIT-19 (RANDOM DISTRESS TYPE	2/89 SAFETY- S IN SECTION EYED- BE SURVEYN PCI BETWEEN	CCI 64 RADRAINAGE- DRAINAGE- DRAINAGE JEAN SIABS JEAN	TING- GOOD SHOULDERS- SURVEYED- 21 SLABS DENSITY-PCT 100.00 33.33 4.76 19.04 14.28 14.28 21 SLABS DENSITY-PCT	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49 DEDUCT-VALUE 12.0 14.2 10.6 13.2 23.2 1.5 SAMPLE PCI- 80 DEDUCT-VALUE		
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-14 (RANDOM DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH SAMPLE UNIT-19 (RANDOM	2/89 SAFETY- S IN SECTION EYED- BE SURVEYE PCI BETWEEN) SEVERITY HIGH LOW MEDIUM LOW MEDIUM LOW SEVERITY MEDIUM LOW SEVERITY	CCI= 64 RA DRAINAGE- DRAIN	TING= GOOD SHOULDERS- SURVEYED= 21 SLABS DENSITY-PCT 100.00 33.33 4.76 19.04 14.28 14.28 21 SLABS DENSITY-PCT 100.00	OVERALL- 21 4 18 15.0 SAMPLE PCI- 49 DEDUCT-VALUE 12.0 14.2 10.6 13.2 23.2 1.5 SAMPLE PCI- 80 DEDUCT-VALUE 7.0		

SAMPLE UNIT-3 (RANDOM)	S	AMPLE SI	ZE-	18 SLABS	SAMPLE PCI- 54
DISTRESS TYPE	SEVERITY	QUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
62 CORNER BR	LOW	1	SLABS	5.55	4.5
65 JT SEAL DAMAGE	MEDIUM	18	SLABS	100.00	7.0
67 LG PATCH/UTIL	LOW	5	SLABS	27.77	12.7
67 LG PATCH/UTIL	MEDIUM	2	SLABS	11.11	17.6
63 LINEAR CR	LOW	5	SLABS	27.77	16.3
66 SMALL PATCH	LOW	7	SLABS	38.88	5.5
62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 66 SMALL PATCH 66 SMALL PATCH	MEDIUM	1	SLABS	5.55	2.9
SAMPLE UNIT-8 (RANDOM)	S	AMPLE SI	ZE-	21 SLABS	SAMPLE PGI- 74
DISTRESS TYPE 62 CORNER BR 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 66 SMALL PATCH	SEVERITY	QUANTI	TY	DENSITY-PCT	DEDUCT-VALUE
62 CORNER BR	LOW	1	SLABS	4.76	3.7
75 CORNER SPALLING	LOW	1	SLABS	4.76	1.8
74 JOINT SPALLING	LOW	1	SLABS	4.76	2.1
65 JT SEAL DAMAGE	MEDIUM	21	SLABS	100.00	7.0
67 LG PATCH/UTIL	LOW	5	SLABS	23.80	11.5
66 SMALL PATCH	LOW	5	SLABS	23.80	2.9
66 SMALL PATCH	MEDIUM	1	SLABS	4.76	2.5
EXTRAPOLATED DISTRESS Q					
DISTRESS TYPE 62 CORNER BR 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 66 SMALL PATCH 66 SMALL PATCH	CETTEDTTV	OUANTT	mv.	DEMOTTY DOT	DEDITOR VALUE
OTSIKESS TIRE	PEARKITI	QUANTI	CIADO	DENSII1-101	DEDUCI-VALUE
75 CODNED CDALLING	LOW	TT	CLADO	1 1 6	0.6
75 CORNER SPALLING	LOW	5	STADS	1 16	0.4
CE IT CEAL DAMACE	LUW	110	CIADO	1.13	12.0
OF IT CEAL DAMAGE	ULGU	112	SLADS	77.72	7.0
67 10 DATOL GITTI	MEDIUM	140	SLADS	74.07	1, 6
67 LG PAICH/UIIL	LOW	149	STABS	34.49	14.6
67 LG PAICH/UIIL	WEDIOM	10	STABS	3.70	8.8
63 LINEAR CR	LOW	48	SLABS	11.11	9.1
63 LINEAR CR	WEDIUM	16	SLABS	3.70	9.2
66 SMALL PATCH	LOW	80	SLABS	18.51	2.0
66 SMALL PATCH	MEDIUM	11	SLABS	2.54	1.3
*** PERCENT OF DED	UCT VALUES	BASED C	N DIS	TRESS MECHANI	[SM ***
LOAD					
CLIMATE/DURABILITY R					
OTHER R	ELATED DIS	TRESSES	- 41	49 PERCENT D	DEDUCT VALUES
	.52.125 515	TREBUBE	-	. TO LEMOBILE	ABOUT VILLOUS.
BRANCH NAME - SOUTH R	AMP TAXIWA	·Υ		SLAB LE	ENGTH - 12.5 LF
BRANCH NUMBER - T22B				SLAB WI	DTH - 20.0 LF
SECTION NUMBER - 1				NUMBER	OF SLABS - 366
INSPECTION DATE - 06/02					
CONDITION- RIDING- S					
			:	PUOULDEKS -	
TOTAL NUMBER OF SAMPLES		1.14 see			20
NUMBER OF SAMPLES SURVE	.1 LU=				3

RECOMMENDED SAMPLES TO STANDARD DEVIATION OF E	S SURVEYED=	19 18.1			
SAMPLE UNIT-11 (RANDOM)	;	SAMPLE SIZE-	18 SLABS	SAMPLE PCI- 73	
DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	SEVERITY HIGH HIGH LOW N/A LOW	QUANTITY 1 SLAH 18 SLAH 2 SLAH 14 SLAH 1 SLAH	DENSITY-PCT S 5.55 S 100.00 S 11.11 S 77.77 S 5.55	DEDUCT-VALUE 5.5 12.0 9.1 11.6 0.7	
SAMPLE UNIT-17 (RANDOM)	,	SAMPLE SIZE-	18 SLABS	SAMPLE PCI- 38	
DISTRESS TYPE 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 72 SHATTERED SLAB 73 SHRINKAGE CR	SEVERITY HIGH LOW MEDIUM LOW LOW N/A	QUANTITY 18 SLAI 12 SLAI 4 SLAI 9 SLAI 1 SLAI	DENSITY-PCT S 100.00 S 66.66 S 22.22 S 50.00 S 5.55 S 50.00	DEDUCT-VALUE 12.0 21.8 29.7 12.2 11.7 7.4	
SAMPLE UNIT-4 (RANDOM)	;	SAMPLE SIZE-	18 SLABS	SAMPLE PCI- 64	
DISTRESS TYPE 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	HIGH LOW N/A LOW	18 SLAI 13 SLAI 17 SLAI 2 SLAI	S 100.00 S 72.22 S 94.44 S 11.11	DEDUCT-VALUE 12.0 22.0 13.7 1.1	
EXTRAPOLATED DISTRESS C					
DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH	MEDIUM LOW LOW N/A	27 SLAI 61 SLAI 7 SLAI 271 SLAI	7.37 5 16.66 5 1.91 5 74.04	15.4 6.0 4.7 11.1	
*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES - 55.74 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES - 16.60 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES - 27.66 PERCENT DEDUCT VALUES.					
BRANCH NAME - SOUTH C BRANCH NUMBER - A01B SECTION NUMBER - 1			SLAB L SLAB W	ENGTH - 12.5 LF TDTH - 20.0 LF OF SLABS - 716	

INSPECTION DATE - 06/02/89 PCI= 83 RATING= VERY GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 23 NUMBER OF SAMPLES SURVEYED= 3 RECOMMENDED SAMPLES TO BE SURVEYED= 15 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 7.0 SAMPLE UNIT-12 (RANDOM) SAMPLE SIZE- 20 SLABS SAMPLE PCI- 82							
DISTRESS TYPE 74 JOINT SPALLING 67 LG PATCH/UTIL 66 SMALL PATCH	SEVERITY HIGH LOW LOW	QUANTIT 1 : 3 : 2 :	TY SLABS SLABS SLABS	DENSITY-PCT 5.00 15.00 10.00	DEDUCT-VALUE 13.2 8.0 1.1		
SAMPLE UNIT-18 (RANDON	1)	SAMPLE SI	ZE-	20 SLABS	SAMPLE PCI- 91		
DISTRESS TYPE 74 JOINT SPALLING 63 LINEAR CR 66 SMALL PATCH	SEVERITY LOW LOW LOW	QUANTIY 2 S 1 S	TY SLABS SLABS SLABS	DENSITY-PCT 10.00 5.00 5.00	DEDUCT-VALUE 3.5 4.9 0.6		
SAMPLE UNIT-6 (RANDON	ſ)	SAMPLE SI	ZE-	20 SLABS	SAMPLE PCI- 77		
DISTRESS TYPE 74 JOINT SPALLING 63 LINEAR CR	SEVERITY HIGH LOW	QUANTIT 1 S 5 S	TY SLABS SLABS	DENSITY-PCT 5.00 25.00	DEDUCT-VALUE 13.2 15.5		
EXTRAPOLATED DISTRESS DISTRESS TYPE	QUANTITIES SEVERITY	FOR SECT	ION- TY	DENSITY-PCT	DEDUCT-VALUE		
74 JOINT SPALLING 74 JOINT SPALLING 67 LG PATCH/UTIL 63 LINEAR CR 66 SMALL PATCH	HIGH LOW LOW LOW LOW	24 3 24 3 36 3 72 3	SLABS SLABS SLABS SLABS SLABS	3.35 3.35 5.02 10.05 5.02	9.8 1.7 3.1 8.5 0.6		
*** PERCENT OF DI LOAD CLIMATE/DURABILITY	EDUCT VALUE RELATED DI RELATED DI	S BASED OF STRESSES •	N DIST - 35.	TRESS MECHANI 86 PERCENT D 00 PERCENT D	SM *** EDUCT VALUES.		
BRANCH NAME - MAIN O BRANCH NUMBER - A02E SECTION NUMBER - 1	PERATIONAL B	. APRON		SLAB WI NUMBER	NGTH - 25.0 LF DTH - 25.0 LF OF SLABS - 2592		
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO	02/89 SAFETY-' ES IN SECTI VEYED=	PCI= 45 DRAINAGE ON=	RAT	ING- FAIR			

STANDARD DEVIATION OF P	CI BETWEEN	RANDOM UNITS	SURVEYED-	18.3
SAMPLE UNIT-104(RANDOM)	S	AMPLE SIZE-	20 SLABS	SAMPLE PCI- 24
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	HIGH	1 SLABS	5.00	5.1
65 JT SEAL DAMAGE	HIGH	20 SLABS	100.00	12.0
75 CORNER SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 70 SCALING/CRAZING	HIGH	2 SLABS	10.00	28.5
67 LG PATCH/UTIL	MEDIUM	2 SLABS	10.00	16.8
63 LINEAR CR	MEDIUM	6 SLABS	30.00	35.0
70 SCALING/CRAZING	LOW	19 SLABS	95.00	16.5
73 SHRINKAGE CR	N/A	1 SLABS	5.00	1.0
66 SMALL PATCH	HIGH	2 SLABS	10.00	10.8
73 SHRINKAGE CR 66 SMALL PATCH 66 SMALL PATCH	MEDIUM	4 SLABS	20.00	9.5
SAMPLE UNIT-14 (RANDOM)	S			SAMPLE PCI- 48
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING	LOW	1 SLABS	4.00	2.0
65 JT SEAL DAMAGE	HIGH	25 SLABS	100.00	12.0
63 LINEAR CR	HIGH	3 SLABS	12.00	29.0 11.8
63 LINEAR CR	LOW	4 SLABS	16.00	11.8
/U SCALING/CRAZING	LOW	25 SLABS	100.00	17.0
66 SMALL PATCH	LOW	2 SLABS	8.00	1.0
SAMPLE UNIT-23 (RANDOM)	S	AMPLE SIZE-	20 SLABS	SAMPLE PCI- 65
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING	LOW	1 SLABS	5.00	2.2
65 JT SEAL DAMAGE	HIGH	20 SLABS	100.00	12.0
63 LINEAR CR	LOW	4 SLABS	20.00	13.7
70 SCALING/CRAZING	LOW	18 SLABS	90.00	16.0
/3 SHRINKAGE CR	N/A	3 SLABS	15.00	2.1
74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR 66 SMALL PATCH	TOM	2 SLABS	10.00	1.1
SAMPLE UNIT-32 (RANDOM)	S	SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 70
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	
74 JOINT SPALLING	LOW	2 SLABS	10.00	3,5
65 JT SEAL DAMAGE	HIGH	20 SLABS	100.00	12.0
	LOW		5.00	4.9
70 SCALING/CRAZING			10,00	
		14 SLABS		10.7
66 SMALL PATCH	LOW	3 SLABS	15.00	1.6
SAMPLE UNIT-41 (RANDOM)	S	AMPLE SIZE-	25 SLABS	SAMPLE PCI- 21
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING			4.00	
65 JT SEAL DAMAGE	HIGH		100.00	12.0
67 LG PATCH/UTIL		1 SLABS		2.5
63 LINEAR CR				

63 LINEAR CR	LOW	5 SLABS	20.00	13.7
63 LINEAR CR	MEDIUM	6 SLABS	24.00	31.0
70 SCALING/CRAZING	LOW	23 SLABS	92.00	16.2
63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR	N/A	7 SLABS	28.00	3.9
66 SMALL PATCH	LOW	6 SLABS	24.00	3.0
SAMPLE UNIT-5 (RANDOM)		SAMPLE SIZE-	25 SLABS	SAMPLE PGI- 15
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR 66 SMALL PATCH	SEVERITY	OUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING	HIGH	1 SLABS	4.00	11.5
65 JT SEAL DAMAGE	HIGH	25 SLABS	100.00	12.0
67 LG PATCH/UTIL	HIGH	1 SLABS	4.00	15.0
67 LG PATCH/UTIL	LOW	1 SLABS	4.00	2.5
63 LINEAR CR	HIGH	9 SLABS	36.00	55.3
63 LINEAR CR	LOW	4 SLABS	16.00	11.8
63 LINEAR CR	MEDIUM	5 SLABS	20.00	28.2
70 SCALING/CRAZING	LOW	24 SLABS	96.00	16.6
73 SHRINKAGE CR	N/A	1 SLABS	4.00	1.0
66 SMALL PATCH	HIGH	1 SLABS	4.00	5.5
SAMPLE UNIT-50 (RANDOM)		SAMPLE SIZE-	25 SLABS	SAMPLE PCI- 46
DICTORGE TYPE	e etten tat	7 OHANITTV	DENCITY DOT	DEDUCT UNITE
DISTRESS TYPE 65 JT SEAL DAMAGE	SEVERILI	. QUANTITI	100 00	10 O
67 IC DATEL DATAGE	uren	CO ALADO	16.00	12.0
67 LG PAIGH/UILL	LOW	4 SLADS	10.00	0.3
67 LG PAICH/UILL	MEDIUM	1 SLABS	4.00	9.5
63 LINEAR CR	HIGH	1 SLADS	4.00	14.0
63 LINEAR CR	LOW	3 SLABS	12.00	9.7
65 LINEAR CR	MEDIUM	3 SLABS	12.00	21.0
67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING	TOM	II SLABS	44.00	11.5
SAMPLE UNIT-59 (RANDOM)				
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING		1 SLABS		13.2
65 JT SEAL DAMAGE				
67 LG PATCH/UTIL			15.00	
63 LINEAR CR				
70 SCALING/CRAZING			40.00	
73 SHRINKAGE CR			45.00	
66 SMALL PATCH				
SAMPLE UNIT-68 (RANDOM)				
SAMPLE UNII-00 (RANDOM)		SAMPLE SIZE-	20 SLABS	SAMPLE POI- 39
DISTRESS TYPE	SEVERITY	Y QUANTITY	DENSITY-PCT	DEDUCT-VALUE
DISTRESS TYPE 74 JOINT SPALLING	MEDIUM	1 SLABS	5.00	4.5
65 JT SEAL DAMAGE	HIGH	20 SLABS	100.00	12.0
67 LG PATCH/UTIL			10.00	6.0
63 LINEAR CR		1 SLABS		16.2
70 SCALING/CRAZING				15.2
73 SHRINKAGE CR		3 SLABS		2.1
	LOW	5 SLABS		3.2

SAMPLE UNIT-77 (RANDOM)		SAMPLE SIZE-	25 SLABS	SAMPLE PCI- 37
NTOTOFCO TVDC	CENTRATTV	OIIANTTTV	DENCTTY DOT	DEDUCT UATUE
DISTRESS TYPE	IOU	1 CIADO	DENSIII-FGI	DEDOCT-ANTOR
62 CORNER BR 65 JT SEAL DAMAGE	LUW	STADS I	100 00	12.0
65 JI SEAL DAMAGE	nich	2. SLADS 2. CT ADS	100.00	12.0
63 LINEAR CR	UTGU	Z SLADS	0.00	22.3
63 LINEAR CR	TOM	7 SLADS	28.00	10.4
53 LINEAR UK	LEDION	2 SLADS	0.00	16.5
/U SUALING/UKAZING	LUW	23 SLABS	92.00	16.2
/3 SHRINKAGE CR	N/A	8 SLABS	32.00	4.5
66 SMALL PATCH	HIGH	1 SLABS	4.00	5.5
63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR 66 SMALL PATCH 66 SMALL PATCH	LUW	I SLABS	4.00	0.5
SAMPLE UNIT-86 (RANDOM)	:	SAMPLE SIZE-		
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	HIGH	1 SLABS	4.00	4.2
75 CORNER SPALLING	LOW	1 SLABS	4.00	1.5
74 JOINT SPALLING	HIGH	1 SLABS	4.00	11.5
74 JOINT SPALLING	LOW	1 SLABS	4.00	2.0
65 JT SEAL DAMAGE	HIGH	25 SLABS	100.00	12.0
63 LINEAR CR	HIGH	2 SLABS	8.00	22.3
63 LINEAR CR	MEDIUM	2 SLABS	8.00	16.5
63 LINEAR CR 70 SCALING/CRAZING	T.OW	24 SLABS	96.00	16.6
73 SHRINKAGE CR	N/A	8 SLARS	32.00	4.5
66 SMALL PATCH	LOW	2 SLARS	8 00	1.0
				4.0
SAMPLE UNIT-95 (RANDOM)				
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 70 SCALING/CRAZING 66 SMALL PATCH	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING	HIGH	1 SLABS	5.00	13.2
65 JT SEAL DAMAGE	HIGH	20 SLABS	100.00	12.0
67 LG PATCH/UTIL	LOW	1 SLABS	5.00	3.1
70 SCALING/CRAZING	LOW	19 SLABS	95.00	16.5
66 SMALL PATCH	HIGH	1 SLABS	5.00	6.3
66 SMALL PATCH	MEDIUM	1 SLABS	5.00	2.7
EXTRAPOLATED DISTRESS Q	UANTITIES	FOR SECTION-		
DISTRESS TYPE				
	SEVERTTY	OHANTITY	DENSITY-PCT	DEDUCT-VALUE
		•		
62 CORNER BR	LOW	10 SLABS	0.38	0.2
62 CORNER BR 75 CORNER SPALLING	LOW HIGH	10 SLABS 19 SLABS	0.38 0.73	0.2 0.8
62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING	LOW HIGH LOW	10 SLABS 19 SLABS 10 SLABS	0.38 0.73 0.38	0.2 0.8 0.1
62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING	LOW HIGH LOW HIGH	10 SLABS 19 SLABS 10 SLABS 38 SLABS	0.38 0.73 0.38 1.46	0.2 0.8 0.1 4.7
62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING	LOW HIGH LOW HIGH LOW	10 SLABS 19 SLABS 10 SLABS 38 SLABS 58 SLABS	0.38 0.73 0.38 1.46 2.23	0.2 0.8 0.1 4.7 1.3
62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING	LOW HIGH LOW HIGH LOW MEDIUM	10 SLABS 19 SLABS 10 SLABS 38 SLABS 58 SLABS 10 SLABS	0.38 0.73 0.38 1.46 2.23 0.38	0.2 0.8 0.1 4.7 1.3 0.3
62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE	LOW HIGH LOW HIGH LOW MEDIUM HIGH	10 SLABS 19 SLABS 10 SLABS 38 SLABS 58 SLABS 10 SLABS 2592 SLABS	0.38 0.73 0.38 1.46 2.23 0.38 100.00	0.2 0.8 0.1 4.7 1.3 0.3 12.0
62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL	LOW HIGH LOW HIGH LOW MEDIUM HIGH HIGH	10 SLABS 19 SLABS 10 SLABS 38 SLABS 58 SLABS 10 SLABS 2592 SLABS 29 SLABS	0.38 0.73 0.38 1.46 2.23 0.38 100.00 1.11	0.2 0.8 0.1 4.7 1.3 0.3 12.0 4.4
62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL	LOW HIGH LOW HIGH LOW MEDIUM HIGH HIGH LOW	10 SLABS 19 SLABS 10 SLABS 38 SLABS 58 SLABS 10 SLABS 2592 SLABS 29 SLABS 115 SLABS	0.38 0.73 0.38 1.46 2.23 0.38 100.00 1.11 4.43	0.2 0.8 0.1 4.7 1.3 0.3 12.0 4.4 2.7
62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL	LOW HIGH LOW HIGH LOW MEDIUM HIGH HIGH	10 SLABS 19 SLABS 10 SLABS 38 SLABS 58 SLABS 10 SLABS 2592 SLABS 29 SLABS	0.38 0.73 0.38 1.46 2.23 0.38 100.00 1.11 4.43 1.11	0.2 0.8 0.1 4.7 1.3 0.3 12.0 4.4

63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR 66 SMALL PATCH 66 SMALL PATCH 66 SMALL PATCH *** PERCENT OF DI						
LOAD CLIMATE/DURABILITY OTHER	RELATED DIS	TRESSES =	= 49. = 11.	02 PERCENT D 74 PERCENT D	EDUCT VALUES. EDUCT VALUES.	
BRANCH NAME - MAIN OPER APRON-WEST EDGE SLAB LENGTH - 25.0 LF BRANCH NUMBER - A03B SLAB WIDTH - 25.0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 258						
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	02/89 F SAFETY- ES IN SECTION VEYED= D BE SURVEYE PCI BETWEEN	CI= 77 DRAINAGE- N= CD= RANDOM U	RAT - S JNITS	ING= VERY G HOULDERS- SURVEYED=	OOD OVERALL- 14 3 7 4.0	
SAMPLE UNIT-12 (RANDON						
DISTRESS TYPE 65 JT SEAL DAMAGE 63 LINEAR CR	SEVERITY MEDIUM LOW	QUANTIT 20 S 8 S	TY SLABS SLABS	DENSITY-PCT 100.00 40.00	DEDUCT-VALUE 7.0 19.0	
SAMPLE UNIT-3 (RANDON	() S	AMPLE SIZ	ZE-	20 SLABS	SAMPLE PCI- 72	
DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 66 SMALL PATCH 36 SMALL PATCH	MEDIUM	1 5	SLABS	5.00	3.3	
SAMPLE UNIT-7 (RANDON						
DISTRESS TYPE 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 66 SMALL PATCH	LOW MEDIUM MEDIUM LOW LOW	2 S 1 S 20 S 2 S 7 S	SLABS SLABS SLABS SLABS SLABS	10.00 5.00 100.00 10.00 35.00	3.5 4.5 7.0	

EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-

DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 66 SMALL PATCH	MEDIUM LOW MEDIUM LOW LOW	4 13 9 258 34 34	SLABS SLABS SLABS SLABS SLABS SLABS	1.55 5.03 3.48 100.00 13.17 13.17	DEDUCT-VALUE 1.3 2.2 3.3 7.0 7.2 10.3 1.6 0.8	
*** PERCENT OF DED LOAD R CLIMATE/DURABILITY R OTHER R	ELATED DI ELATED DI	STRESSES STRESSES	= 30 = 20	.56 PERCENT D	DEDUCT VALUES. DEDUCT VALUES.	
BRANCH NAME - NORTH OPERATIONAL APRON SLAB LENGTH - 25.0 LF BRANCH NUMBER - A04B SLAB WIDTH - 20.0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 1238						
INSPECTION DATE - 06/02/89 PCI= 73 RATING= VERY GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 56 NUMBER OF SAMPLES SURVEYED= 8 RECOMMENDED SAMPLES TO BE SURVEYED= 19 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 11.0						
SAMPLE UNIT-19 (RANDOM)						
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR	SEVERITY MEDIUM HIGH HIGH MEDIUM LOW N/A	QUANT: 2 20 2 2 3 4	ITY SLABS SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 10.00 100.00 10.00 10.00 15.00 20.00	DEDUCT-VALUE 8.0 12.0 26.0 19.0 5.6 2.9	
SAMPLE UNIT-21 (RANDOM)		SAMPLE S	IZE-	20 SLABS	SAMPLE PCI- 76	
DISTRESS TYPE 65 JT SEAL DAMAGE 70 SCALING/CRAZING 73 SHRINKAGE CR	SEVERITY HIGH LOW N/A	QUANT: 20 16 3	ITY SLABS SLABS SLABS	DENSITY-PCT 100.00 80.00 15.00	DEDUCT-VALUE 12.0 15.2 2.1	
SAMPLE UNIT-30 (RANDOM)		SAMPLE S	IZE-	25 SLABS	SAMPLE PCI- 84	
DISTRESS TYPE 65 JT SEAL DAMAGE 70 SCALING/CRAZING 73 SHRINKAGE CR	HIGH LOW N/A	25 5 3	SLABS SLABS SLABS	100.00 20.00 12.00	12.0 7.0 1.7	
SAMPLE UNIT-39 (RANDOM)		SAMPLE S.	LZL-	ZU SLABS	SAMPLE PCI- 82	

DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 70 SCALING/CRAZING	SEVERITY LOW HIGH LOW	QUANT: 1 20 2	ITY SLABS SLABS SLABS	DENSITY-PCT 5.00 100.00 10.00	DEDUCT-VALUE 1.9 12.0 4.0
SAMPLE UNIT-44 (RANDOM)					
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 73 SHRINKAGE CR 66 SMALL PATCH	SEVERITY HIGH LOW MEDIUM LOW HIGH N/A LOW	QUANT 1 1 1 20 1	SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 5.00 5.00 5.00 5.00 100.00 5.00 5.00	DEDUCT-VALUE 5.1 1.9 3.3 2.2 12.0 1.0 0.6
SAMPLE UNIT-46 (RANDOM)		SAMPLE S	IZE-	25 SLABS	SAMPLE PCI- 74
DISTRESS TYPE 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 73 SHRINKAGE CR	SEVERITY HIGH LOW MEDIUM HIGH N/A	QUANT: 1 2 1 25	SLABS SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 4.00 8.00 4.00 100.00 4.00	DEDUCT-VALUE 11.5 3.0 3.8 12.0 1.0
SAMPLE UNIT-52 (RANDOM)		SAMPLE S	IZE-	25 SLABS	SAMPLE PCI- 70
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE	SEVERITY LOW MEDIUM LOW HIGH LOW MEDIUM HIGH	QUANT: 1 1 1 3 2 25	SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 4.00 4.00 4.00 4.00 12.00 8.00 100.00	DEDUCT-VALUE 1.5 2.2 1.5 11.5 4.0 6.8 12.0
SAMPLE UNIT-8 (RANDOM)		SAMPLE S	IZE-	20 SLABS	SAMPLE PCI - 65
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR EXTRAPOLATED DISTRESS QU	LOW MEDIUM LOW MEDIUM LOW N/A	2 20 2 1 16 6	SLABS SLABS SLABS SLABS SLABS SLABS	10.00 100.00 10.00 5.00 80.00 30.00	3.5 7.0 8.5 11.6 15.2
•					DDDIIGE WAYNE
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 75 CORNER SPALLING	HIGH LOW MEDIUM	QUANT. 7 21 14	SLABS SLABS SLABS SLABS	0.56 1.69 1.13	DEDUCT-VALUE 0.6 0.5 0.9

64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 73 SHRINKAGE CR 66 SMALL PATCH	LOW HIGH LOW MEDIUM HIGH HIGH LOW MEDIUM LOW N/A LOW	7 5 14 5 57 5 35 5 1096 5 141 5 14 5 21 5 297 5 127 5	SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS	0.56 1.13 4.60 2.82 88.52 11.38 1.13 1.69 23.99 10.25 0.56	0.2 3.6 2.1 2.7 12.0 7.0 4.5 1.1 4.2 7.9 1.5 0.0
*** PERCENT OF DED LOAD R CLIMATE/DURABILITY R OTHER R	ELATED DIS ELATED DIS	TRESSES =	= 20. = 39.	08 PERCENT DE 34 PERCENT DE	DUCT VALUES. DUCT VALUES.
BRANCH NAME - MAINTEN BRANCH NUMBER - A05B SECTION NUMBER - 1	ANCE APRON			SLAB LEN SLAB WII NUMBER O	GTH - 25.0 LF OTH - 20.0 LF OF SLABS - 164
INSPECTION DATE - 06/02 CONDITION- RIDING- S TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVE RECOMMENDED SAMPLES TO STANDARD DEVIATION OF P	AFETY- IN SECTIO YED= BE SURVEYE CI BETWEEN	DRAINAGE N= D= RANDOM U	- S UNITS	SURVEYED=	0VERALL- 8 2 8 8.4
DISTRESS TYPE 75 CORNER SPALLING 64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR	LOW MEDIUM LOW N/A	3 3 20 3 3 3 1 3	SLABS SLABS SLABS SLABS	15.00 100.00 15.00 5.00	4.7 7.0 11.3 1.0
SAMPLE UNIT-6 (RANDOM) DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	S SEVERITY MEDIUM MEDIUM LOW N/A LOW	AMPLE SI QUANT 1 20 2 1 4	ZE- ITY SLABS SLABS SLABS SLABS	20 SLABS DENSITY-PCT 5.00 100.00 10.00 5.00 20.00	SAMPLE PCI- 81 DEDUCT-VALUE 4.5 7.0 8.5 1.0 2.2
EXTRAPOLATED DISTRESS Q DISTRESS TYPE	UANTITIES	FOR SECT	ION-		

75 CORNER SPALLING	LOW	4 SLABS	3 2.43	0.8
64 DURABILITY CR	LOW	4 SLABS	2.43	
74 JOINT SPALLING	HIGH	4 SLABS	2.43	7.5
7/ 70717 00477 7170	TOTT	10 01 40	7 71	0.0
74 JOINT SPAILING	MEDITIM	4 STARS	2 43	2.4
65 IT SEAL DAMAGE	MEDIUM	164 STARS	100.00	7.0
63 TINEAD CD	TOU	21 STARS	12 80	10.1
73 CUDINDACE CD	NI/A	2 STARG	2 / 97	10.1
74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	IOU	16 CTARG	9.07	1.0
OU SIMLE TATOM	LOW	TO DIVIDE	5.75	1.0
*** PERCENT OF I	EDUCT VALUE	ES BASED ON DIS	STRESS MECHANI	SM ***
		ISTRESSES = 29		
CLIMATE/DURABILITY				
OTHER	RELATED D	ISTRESSES = 45	5.99 PERCENT D	EDUCT VALUES.
BRANCH NAME - SOUTH	i EDGE-HANG	AR ACCESS	SLAB LE	NGTH - 15.0 LF
BRANCH NUMBER - AO	ьВ		SLAB WI	DTH - 15.0 LF
SECTION NUMBER - 1			NUMBER	OF SLABS - 61
NODECTION DATE OF	/02 /00	DCT 03 D/	ATTNOL VEDV C	
INSPECTION DATE - 06, CONDITION- RIDING-				
COTAL NUMBER OF SAMPI			SHOOLDERS-	
		I OIV=		3
NUMBER OF SAMPLES SUE		n avinvinium		1
RECOMMEND ALL SAMPLE	UNITS TO B	E SURVEYED.		
SAMPLE UNIT-2 (RANDO	OM)	SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 83
DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR	SEVERIT	Y QUANTITY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	MEDIUM	1 SLABS	5.00	3.3
65 JT SEAL DAMAGE	MEDIUM	20 SLABS	100.00	7.0
63 LINEAR CR	LOW	1 SLABS	s 5.00	4.9
73 SHRINKAGE CR	N/A	2 SLABS	3 10.00	1.5
EXTRAPOLATED DISTRESS	QUANTITIE	S FOR SECTION-		•
DISTRESS TYPE	SEVERTT	Y OHANTITY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPAILING	MEDITIM	3 STARS	4 91	3.2
65 IT SEAL DAMAGE	MEDILIM	61 STARS	100 00	7.0
63 ITHEAD CD	I UII	S GIVE	2 100.00	4.8
75 CORNER SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR	TOM	S SLADO C CI ADO	9.71	1.4
13 SHKINKAGE OK	n/A	O SLAD	3.03	1,4
*** PERCENT OF I	DEDUCT VALU	ES BASED ON DIS	STRESS MECHANT	SM ***
LOAD				
CLIMATE/DURABILITY				
OTHER				
OTHER	KHERIED D.	rotundana - 7(J.OJ IEROENI D	PROOF AUTORS.
BRANCH NAME - HANGA				
		BLDG 119)		
		BLDG 119)	SLAB WI	DTH - 15.0 LF
BRANCH NUMBER - AOT SECTION NUMBER - 1		BLDG 119)	SLAB WI	

INSPECTION DATE - 06/02/89 CONDITION- RIDING- SAFI TOTAL NUMBER OF SAMPLES IN NUMBER OF SAMPLES SURVEYED RECOMMENDED SAMPLES TO BE STANDARD DEVIATION OF PCI	9 PCI- 90 RAT ETY- DRAINAGE- SI N SECTION- D- SURVEYED- BETWEEN RANDOM UNITS	ING-EXCELLEN HOULDERS-C SURVEYED-	11
SAMPLE UNIT-5 (RANDOM)			
DISTRESS TYPE ST 75 CORNER SPALLING MI 65 JT SEAL DAMAGE MI	EDIUM 1 SLABS EDIUM 15 SLABS	6.66 100.00	4.9 7.0
SAMPLE UNIT-9 (RANDOM)			
DISTRESS TYPE STATE STATE OF THE STATE OF TH	EVERITY QUANTITY EDIUM 20 SLABS /A 1 SLABS	DENSITY-PCT 100.00 5.00	DEDUCT-VALUE 7.0 1.0
EXTRAPOLATED DISTRESS QUA	NTITIES FOR SECTION-		
	T VALUES BASED ON DIST	RESS MECHANISI	y ***
LOAD REL CLIMATE/DURABILITY REL OTHER REL	ATED DISTRESSES - 72. ATED DISTRESSES - 27.	16 PERCENT DE	DUCT VALUES. DUCT VALUES.
BRANCH NAME - HANGAR ACE BRANCH NUMBER - A09B SECTION NUMBER - 1	CESS (BLDG 184)		GTH - 20.0 LF TH - 20.0 LF F SLABS - 57
INSPECTION DATE - 06/02/8 CONDITION- RIDING- SAF TOTAL NUMBER OF SAMPLES IN NUMBER OF SAMPLES SURVEYERECOMMEND ALL SAMPLE UNIT	ETY- DRAINAGE- S N SECTION- D- S TO BE SURVEYED.	ING- VERY GO	OD OVERALL- 4 2
SAMPLE UNIT-1 (RANDOM)			
DISTRESS TYPE STATE TO THE STATE OF THE STAT	EVERITY QUANTITY OW 2 SLABS OW 1 SLABS EDIUM 1 SLABS EDIUM 29 SLABS	DENSITY-PCT 6.89 3.44 3.44 100.00	DEDUCT-VALUE 2.6 1.8 3.3 7.0
SAMPLE UNIT-2 (RANDOM)			

DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR EXTRAPOLATED DISTRESS (LOW LOW LOW	1 S 9 S 20 S 3 S	SLABS SLABS SLABS SLABS	5.00 45.00 100.00 15.00	DEDUCT-VALUE 1.9 10.3 7.0 11.3
DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR *** PERCENT OF DELLOAD	SEVERITY LOW LOW MEDIUM LOW DUCT VALUES	QUANTIT 3 S 12 S 57 S 3 S BASED ON	TY SLABS SLABS SLABS SLABS	DENSITY-PCT 5.26 21.05 100.00 5.26 PRESS MECHANI	[SM ***
CLIMATE/DURABILITY I	RELATED DIS	STRESSES =	45.	00 PERCENT I	DEDUCT VALUES.
BRANCH NAME - MAINTER BRANCH NUMBER - A10B SECTION NUMBER - 1				DIMID W.	
INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVI RECOMMENDED SAMPLES TO STANDARD DEVIATION OF 1	2/89 F SAFETY- S IN SECTION EYED= BE SURVEYE PCI BETWEEN	PCI= 63 DRAINAGE- DN= ED= I RANDOM U	RAT · S	TING- GOOD HOULDERS -	OVERALL- 13 3
SAMPLE UNIT-11 (RANDOM)					4.9
) S	SAMPLE SIZ			
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE	SEVERITY HIGH	QUANTIT	ZE- TY SLABS	20 SLABS DENSITY-PCT 10.00	SAMPLE PCI- 63 DEDUCT-VALUE 9.0
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE SAMPLE UNIT-3 (RANDOM)	SEVERITY HIGH MEDIUM HIGH HIGH MEDIUM MEDIUM	QUANTIT 2 S 1 S 2 S 1 S 2 S	ZE- TY SLABS SLABS SLABS SLABS SLABS SLABS	20 SLABS DENSITY-PCT 10.00 5.00 5.00 10.00 5.00	SAMPLE PCI- 63 DEDUCT-VALUE 9.0 3.3 8.3 20.8 4.5 7.0
75 CORNER SPALLING 75 CORNER SPALLING 64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE	SEVERITY HIGH MEDIUM HIGH MEDIUM MEDIUM MEDIUM SEVERITY HIGH LOW MEDIUM HIGH MEDIUM	QUANTIT 2 S 1 S 2 S 20 S CAMPLE SIZ QUANTIT 1 S 1 S 2 S	ZE- TY SLABS	20 SLABS DENSITY-PCT 10.00 5.00 10.00 5.00 100.00 20 SLABS DENSITY-PCT 5.00 5.00 15.00 25.00 100.00	SAMPLE PCI- 63 DEDUCT-VALUE 9.0 3.3 8.3 20.8 4.5 7.0 SAMPLE PCI- 60 DEDUCT-VALUE 5.1 1.9 9.7 32.2 7.0

DISTRESS TYPE 75 CORNER SPALLING 64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL EXTRAPOLATED DISTRESS	HIGH LOW HIGH MEDIUM MEDIUM LOW	2 SLABS 1 SLABS 2 SLABS 1 SLABS 20 SLABS 1 SLABS	10.00 5.00 10.00 5.00 100.00 5.00	9.0 1.8 20.8 4.5 7.0 3.1		
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 75 CORNER SPALLING 64 DURABILITY CR 64 DURABILITY CR 74 JOINT SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 73 SHRINKAGE CR *** PERCENT OF DE LOAD CLIMATE/DURABILITY OTHER	N/A DUCT VALUES RELATED DIS RELATED DIS	4 SLABS BASED ON DIS TRESSES = 19	1.50 TRESS MECHANI .00 PERCENT D	0.6 SM *** EDUCT VALUES. EDUCT VALUES.		
BRANCH NAME - WARM-UP PAD - TAXIWAY D SLAB LENGTH - 12.5 LF BRANCH NUMBER - A11B SLAB WIDTH - 20.0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 159						
INSPECTION DATE - 06/02/89 PCI= 77 RATING= VERY GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 8 NUMBER OF SAMPLES SURVEYED= 1 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.						
SAMPLE UNIT-5 (RANDOM				SAMPLE PCI- 77		
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	LOW	4 SLABS	20.00	2.2		
EXTRAPOLATED DISTRESS						
DISTRESS TYPE 74 JOINT SPALLING	SEVERITY LOW	QUANTITY 8 SLABS	DENSITY-PCT 5.03	DEDUCT-VALUE 2.2		

65 JT SEAL DAMAGE 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	LOW N/A LOW	8 16 32	SLABS SLABS SLABS	5.03 10.06 20.12	4.9 1.5 2.2
*** PERCENT OF DEI LOAD F CLIMATE/DURABILITY F OTHER F	RELATED DIS RELATED DIS RELATED DIS	TRESSES TRESSES TRESSES	= 21. = 52. = 25.	49 PERCENT D 63 PERCENT D 88 PERCENT D	EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - ARM-DIS BRANCH NUMBER - A12B SECTION NUMBER - 1	SARM PAD -	T/W D		SLAB LE SLAB WI NUMBER	NGTH - 20.0 LF DTH - 20.0 LF OF SLABS - 129
INSPECTION DATE - 06/02/89 PCI= 56 RATING= GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 7 NUMBER OF SAMPLES SURVEYED= 2 RECOMMENDED SAMPLES TO BE SURVEYED= 7 STANDARD DEVIATION OF PCI BETWEEN RANDOM UNITS SURVEYED= 19.0					
SAMPLE UNIT-3 (RANDOM)) S	AMPLE S	ZE-	20 SLABS	SAMPLE PCI- 69
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR	SEVERITY LOW MEDIUM LOW MEDIUM	1 20 2 3	SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 5.00 100.00 10.00 15.00	DEDUCT-VALUE 2.2 7.0 8.5 24.0
SAMPLE UNIT-5 (RANDOM)			ZE-	20 SLABS	SAMPLE PCI- 42
	LOW MEDIUM	1 2 2 4	SLABS SLABS SLABS SLABS	10.00 10.00	DEDUCT-VALUE 4.0 15.0 7.0 4.9 19.0 27.0 2.9 2.2
EXTRAPOLATED DISTRESS (QUANTITIES	FOR SEC	rion-		
62 CORNER BR 74 JOINT SPALLING	LOW MEDIUM LOW LOW MEDIUM	3 6 3 129 10 16	SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS	2.32 100.00 7.75 12.40	DEDUCT-VALUE 1.7 7.6 1.4 7.0 6.9 21.4 18.4

73 SHRINKAGE CR 66 SMALL PATCH	N/A LOW	13 13	SLABS SLABS	10.07 10.07	1.5 1.1	
*** PERCENT OF DED LOAD R CLIMATE/DURABILITY R OTHER R	ELATED DI ELATED DI	STRESSES STRESSES STRESSES	= 83. = 10. = 5.	.58 PERCENT D .45 PERCENT D .97 PERCENT D	EDUCT VALUES. EDUCT VALUES.	
BRANCH NAME - ARM-DISARM ADDITION-T/W D SLAB LENGTH - 20.0 LF BRANCH NUMBER - A13B SLAB WIDTH - 20.0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 35						
INSPECTION DATE - 06/02/89 PCI= 68 RATING= GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL- TOTAL NUMBER OF SAMPLES IN SECTION= 2 NUMBER OF SAMPLES SURVEYED= 2 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED.						
SAMPLE UNIT-1 (RANDOM)		SAMPLE S	IZE-	20 SLABS	SAMPLE PCI- 73	
DISTRESS TYPE 62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE 62 CORNER BR 62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL	SEVERITY	SAMPLE S.	IZE- ITY	DENSITY-PCT	DEDUCT-VALUE	
65 JT SEAL DAMAGE 67 LG PATCH/UTIL	MEDIUM	20 1	SLABS	100.00	7.0 3.1	
63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH 66 SMALL PATCH	MEDIUM N/A LOW MEDIUM	2 3 4 4	SLABS SLABS SLABS	10.00 15.00 20.00 20.00	19.0 2.1 2.2	
EXTRAPOLATED DISTRESS Q	UANTITIES	FOR SEC	rion-			
DISTRESS TYPE 62 CORNER BR 62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR	MEDIUM MEDIUM LOW LOW MEDIUM	1 2 35 6 1 2	SLABS SLABS SLABS SLABS SLABS SLABS	2.85 5.71 100.00 17.14 2.85 5.71	2.1 9.4 7.0 8.9 2.8 12.7	
73 SHRINKAGE CR 66 SMALL PATCH				11.42 14.28		

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES = 51.82 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 13.44 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 34.74 PERCENT DEDUCT VALUES. BRANCH NAME - ARM-DISARM ADDITION-T/W A SLAB LENGTH - 20.0 LF SLAB WIDTH - 20.0 LF BRANCH NUMBER - A14B NUMBER OF SLABS - 39 SECTION NUMBER - 1 _____ INSPECTION DATE - 06/02/89 PCI= 69 RATING= GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-TOTAL NUMBER OF SAMPLES IN SECTION-NUMBER OF SAMPLES SURVEYED= 1 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. ______ SAMPLE UNIT-1 (RANDOM) SAMPLE SIZE- 15 SLABS SAMPLE PCI- 69
 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 74 JOINT SPALLING
 LOW
 1 SLABS
 6.66
 2.6

 65 JT SEAL DAMAGE
 LOW
 15 SLABS
 100.00
 2.0

 63 LINEAR CR
 LOW
 1 SLABS
 6.66
 6.2

 63 LINEAR CR
 MEDIUM
 1 SLABS
 6.66
 14.2

 73 SHRINKAGE CR
 N/A
 1 SLABS
 6.66
 1.1

 66 SMALL PATCH
 MEDIUM
 6 SLABS
 40.00
 15.0
 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-
 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 74 JOINT SPALLING
 LOW
 3 SLABS
 7.69
 2.9

 65 JT SEAL DAMAGE
 LOW
 39 SLABS
 100.00
 2.0

 63 LINEAR CR
 LOW
 3 SLABS
 7.69
 6.8

 63 LINEAR CR
 MEDIUM
 3 SLABS
 7.69
 15.9

 73 SHRINKAGE CR
 N/A
 3 SLABS
 7.69
 1.2

 66 SMALL PATCH
 MEDIUM
 16 SLABS
 41.02
 15.2
 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES - 51.59 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 4.55 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES - 43.86 PERCENT DEDUCT VALUES. BRANCH NAME - ARM-DISARM PAD - T/W A BRANCH NUMBER - A15B SLAB LENGTH - 20.0 LF SLAB WIDTH - 20,0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 134 INSPECTION DATE - 06/02/89 PCI= 74 RATING= VERY GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-TOTAL NUMBER OF SAMPLES IN SECTION-7

NUMBER OF SAMPLES SURVEYED-

RECOMMENDED SAMPLES T	PCI BETWEEN	N RANDOM UNITS	SURVEYED=	6 2.1
SAMPLE UNIT-1 (RANDO			20 SLABS	SAMPLE PCI- 75
DISTRESS TYPE 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB	SEVERITY LOW	QUANTITY 20 SLABS	DENSITY-PCT 100.00	DEDUCT-VALUE 2.0
63 LINEAR CR	LOW	1 SLABS	5.00	4.9
63 LINEAR CR	MEDIUM	1 SLABS	5.00	11.6
72 SHATTERED SLAB	LOW	1 SLABS	5.00	10.9
/3 SHKINKAGE CK	N/A	T STWDS	5.00	1.0
66 SMALL PATCH	LOW	I SLABS	5.00	0.6
SAMPLE UNIT-6 (RANDO	M)	SAMPLE SIZE-	20 SLABS	SAMPLE PCI- 72
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	LOW	1 SLABS	5.00	1.9
63 LINEAR CR	HIGH	1 SLABS	5.00	16.2
63 LINEAR CR	LOW	1 SLABS	5.00	4.9
72 SHATTERED SLAB	LOW	1 SLABS	5.00	10.9
75 CORNER SPALLING 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 66 SMALL PATCH	LOW	1 SLABS	5.00	0.6
EXTRAPOLATED DISTRESS	•	FOR SECTION-		
DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR	SEVERITY	OUANTITY	DENSITY-PCT	DEDUCT-VALUE
75 CORNER SPALLING	LOW	3 SLABS	2.23	0.7
65 JT SEAL DAMAGE	LOW	67 SLABS	50.00	2.0
63 LINEAR CR	HIGH	3 SLABS	2.23	8.6
63 LINEAR CR	LOW	7 SLABS	5.22	5.0
63 LINEAR CR	MEDIUM	3 SLABS	2.23	5.5
/2 SUNTIFIED STWD	LOW	/ SLADS	3,42	11.2
73 SHRINKAGE CR	N/A	3 SLABS	2.23	0.8
66 SMALL PATCH	LOW	7 SLABS	5.22	0.6
*** PERCENT OF D				
LOAD		STRESSES = 88		
CLIMATE/DURABILITY	RELATED DIS	STRESSES = 5	.81 PERCENT I	DEDUCT VALUES.
OTHER	RELATED DIS	STRESSES = 6	.10 PERCENT I	DEDUCT VALUES.
BRANCH NAME - WARM- BRANCH NUMBER - A16 SECTION NUMBER - 1	В		SLAB WI NUMBER	DTH - 20.0 LF OF SLABS - 206
INSPECTION DATE - 06/CONDITION- RIDING- FOTAL NUMBER OF SAMPI NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES T STANDARD DEVIATION OF	O2/89 I SAFETY- LES IN SECTION VEYED= O BE SURVEYED PCI BETWEEN	DRAINAGE- DN- ED- N RANDOM UNITS	TING= VERY O SHOULDERS- SURVEYED=	GOOD OVERALL- 8 2 8
SAMPLE UNIT-2 (RANDO			15 SLABS	SAMPLE PCI- 86

Diampeca mype	CEUED TOW	OTT 4 NUMB	msz	SPNGTMY DOM	DEDUCE HATTIÉ
DISTRESS TIPE	SEVERTII	LIMAUD	LI	DENSITY-PGI	DEDOCI-ANTOR
62 CURNER BR	LOW	1 0	CCAL	6.66	5,4 /, 1
67 LG PAIGN/UILL	LOW	1 0	TADO	0.00	4.1
72 CUDINDACE CD	M /V	1 0	CONTR	6.66	1 1
66 CMAIL DATCH	I OU	1 0	ממאדו ממאדו	6.66	U 8
DISTRESS TYPE 62 CORNER BR 67 LG PATCH/UTIL 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	LOW		TVD9	0.00	
SAMPLE UNIT-7 (RANDOM	I) SA	AMPLE SIZ	E-	20 SLABS	SAMPLE PCI- 77
DISTRESS TYPE 63 LINEAR CR 63 LINEAR CR	SEVERITY	QUANTIT	Ϋ́	DENSITY-PCT	DEDUCT-VALUE
63 LINEAR CR	LOW	2 9	LABS	10.00	8.5
63 LINEAR CR	MEDIUM	2 8	LABS	10.00	19.0
66 SMALL PATCH	LOW	2 5	LABS	10.00	1.1
EXTRAPOLATED DISTRESS	-				
DISTRESS TYPE 62 CORNER BR 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH	SEVERITY	QUANTIT	Ϋ́	DENSITY-PCT	DEDUCT-VALUE
62 CORNER BR	LOW	6 8	LABS	2.91	2.1
67 LG PATCH/UTIL	LOW	6 8	LABS	2.91	1.9
63 LINEAR CR	LOW	18 8	LABS	8.73	7.6
63 LINEAR CR	MEDIUM	12 S	LABS	5.82	12.9
73 SHRINKAGE CR	N/A	6 8	LABS	2.91	0.8
66 SMALL PATCH	LOW	18 8	LABS	8.73	1.0
*** PERCENT OF DE LOAD CLIMATE/DURABILITY OTHER	RELATED DIST	resses = resses =	85	.93 PERCENT D	EDUCT VALUES. EDUCT VALUES.
BRANCH NAME - WARM-U BRANCH NUMBER - A18E SECTION NUMBER - 1	3			SLAB WI NUMBER	DTH - 20.0 LF OF SLABS - 159
INSPECTION DATE - 06/0					
CONDITION- RIDING-					OVERALL.
TOTAL NUMBER OF SAMPLE			•	MOODDERG-	8
NUMBER OF SAMPLES SURV		•			2
RECOMMENDED SAMPLES TO)===			8
STANDARD DEVIATION OF				SURVEYED=	
CAMBIE IINTE 9 (DANIDON					
SAMPLE UNIT-2 (RANDOM					SAMPLE PCI- 66
DISTRESS TYPE	SEVERITY	MPLE SIZ QUANTII	ZE- CY	20 SLABS DENSITY-PCT	SAMPLE PCI- 66 DEDUCT-VALUE
DISTRESS TYPE	SEVERITY	MPLE SIZ QUANTII	ZE- CY	20 SLABS DENSITY-PCT	SAMPLE PCI- 66 DEDUCT-VALUE
DISTRESS TYPE 62 CORNER BR 65 JT SEAL DAMAGE	SEVERITY LOW HIGH	MPLE SIZ QUANTII 1 S 20 S	ZE- CY SLABS SLABS	20 SLABS DENSITY-PCT 5.00 100.00	SAMPLE PCI- 66 DEDUCT-VALUE 4.0 12.0
DISTRESS TYPE 62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL	SEVERITY LOW HIGH LOW	AMPLE SIZ QUANTII 1 S 20 S 2 S	E- CY SLABS SLABS SLABS	20 SLABS DENSITY-PCT 5.00 100.00 10.00	SAMPLE PCI- 66 DEDUCT-VALUE 4.0 12.0 6.0
DISTRESS TYPE 62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR	SEVERITY LOW HIGH LOW LOW LOW	QUANTIT 1 S 20 S 2 S 5 S	E- CY SLABS SLABS SLABS SLABS	20 SLABS DENSITY-PCT 5.00 100.00 10.00 25.00	DEDUCT-VALUE 4.0 12.0 6.0 15.5
DISTRESS TYPE 62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 73 SHRINKAGE CR	SEVERITY LOW HIGH LOW LOW N/A	QUANTIT 1 S 20 S 2 S 5 S	E-CY SLABS SLABS SLABS SLABS SLABS	20 SLABS DENSITY-PCT 5.00 100.00 10.00 25.00 5.00	DEDUCT-VALUE 4.0 12.0 6.0 15.5 1.0
DISTRESS TYPE 62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR	SEVERITY LOW HIGH LOW LOW N/A LOW	QUANTIT 1 S 20 S 2 S 5 S 1 S 8 S	ELABS SLABS SLABS SLABS SLABS SLABS	20 SLABS DENSITY-PCT 5.00 100.00 10.00 25.00 5.00 40.00	DEDUCT-VALUE 4.0 12.0 6.0 15.5 1.0 5.7

SAMPLE UNIT-6 (RANDOM	-)	SAMPLE S	IZE-	20 SLABS	SAMPLE PCI- 71
DISTRESS TYPE 62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR	SEVERTTY	י חוו	ιτγ	DENSTTY PCT	DEDUCT-VALUE
62 CORNER RR	TOU	1	STARS	5 00	4 O
65 IT SEAT DAMAGE	HTCH	20	STARS	100 00	12.0
67 LG PATCH/HTTI.	T.OW	1	STABS	5.00	3 1
63 LINEAR CR	I.OW	4	SLABS	20.00	13.7
73 SHRINKAGE CR	N/A	1	SLABS	5.00	1.0
66 SMALL PATCH	LOW	3	SLABS	15.00	1.6
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SEC	rion-		
DISTRESS TYPE	SEVERITY	QUANT:	ITY	DENSITY-PCT	DEDUCT-VALUE
62 CORNER BR ~	LOW	8	SLABS	5.03	4.0
65 JT SEAL DAMAGE	HIGH	159	SLABS	100.00	12.0
67 LG PATCH/UTIL	LOW	12	SLABS	7.54	4.7
63 LINEAR CR	LOW	36	SLABS	22.64	14.6
73 SHRINKAGE CR	N/A	8	SLABS	5.03	1.0
66 SMALL PATCH	LOW	44	SLABS	27.67	3.6
62 CORNER BR 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 73 SHRINKAGE CR 66 SMALL PATCH 66 SMALL PATCH	MEDIUM	8	SLABS	5.03	2.7
CLIMATE/DURABILITY I		STRESSES	= 28		DEDUCT VALUES.
BRANCH NAME - NORTH (BRANCH NUMBER - A26B SECTION NUMBER - 1		N PAD		SLAB LE SLAB WI NUMBER	CNGTH - 25.0 LF CDTH - 25.0 LF OF SLABS - 97
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION - RIDING - 3	2/89 SAFETY-	PCI= 42	RA'	SLAB LE SLAB WI NUMBER	CNGTH - 25.0 LF CDTH - 25.0 LF OF SLABS - 97 OVERALL-
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES	2/89 SAFETY- S IN SECTI	PCI= 42	RA'	SLAB LE SLAB WI NUMBER	CNGTH - 25.0 LF CDTH - 25.0 LF OF SLABS - 97 OVERALL- 5
SECTION NUMBER - 1 INSPECTION DATE - 06/0: CONDITION- RIDING- TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVI	2/89 SAFETY- S IN SECTI	N PAD PCI= 42 DRAINAGI	RA'	SLAB LE SLAB WI NUMBER	CNGTH - 25.0 LF CDTH - 25.0 LF OF SLABS - 97 OVERALL-
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVI RECOMMEND ALL SAMPLE U	2/89 SAFETY- S IN SECTI EYED= NITS TO BE	PCI= 42 DRAINAGI	RA' E- :	SLAB LE SLAB WI NUMBER FING= FAIR SHOULDERS-	CNGTH - 25.0 LF CDTH - 25.0 LF OF SLABS - 97 OVERALL- 5 2
SECTION NUMBER - 1 INSPECTION DATE - 06/0: CONDITION- RIDING- : TOTAL NUMBER OF SAMPLE: NUMBER OF SAMPLES SURVI RECOMMEND ALL SAMPLE UNIT-2 (RANDOM)	2/89 SAFETY- S IN SECTI EYED= NITS TO BE	PCI= 42 DRAINAGI ON= SURVEYEI SAMPLE S	RATE-	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS	ONGTH - 25.0 LF OF SLABS - 97 OVERALL- 5 2 SAMPLE PCI- 44
SECTION NUMBER - 1 INSPECTION DATE - 06/03 CONDITION- RIDING- STOTAL NUMBER OF SAMPLES SURVINERCOMMEND ALL SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE	2/89 SAFETY- S IN SECTI EYED= NITS TO BE	PCI= 42 DRAINAGI ON= SURVEYEI SAMPLE SI	RA'E- S	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS DENSITY-PCT	ONGTH - 25.0 LF OF SLABS - 97 OVERALL- 5 2 SAMPLE PCI- 44 DEDUCT-VALUE
SECTION NUMBER - 1 INSPECTION DATE - 06/03 CONDITION- RIDING- STOTAL NUMBER OF SAMPLES SURVINERCOMMEND ALL SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE	2/89 SAFETY- S IN SECTI EYED= NITS TO BE	PCI= 42 DRAINAGI ON= SURVEYEI SAMPLE SI	RA'E- S	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS DENSITY-PCT	ONGTH - 25.0 LF OTH - 25.0 LF OF SLABS - 97 OVERALL- 5 2 SAMPLE PCI- 44 DEDUCT-VALUE
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION - RIDING - 3 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVER RECOMMEND ALL SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE 63 LINEAR CR 63 LINEAR CR	2/89 SAFETY- S IN SECTI EYED= NITS TO BE SEVERITY LOW MEDIUM	PCI= 42 DRAINAGION= SURVEYEI SAMPLE ST	RA' E- S IZE- ITY SLABS SLABS	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS DENSITY-PCT 10.00 25.00	CNGTH - 25.0 LF CDTH - 25.0 LF OF SLABS - 97 OVERALL- 5 2 SAMPLE PCI- 44 DEDUCT-VALUE 8.5 31.8
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVI RECOMMEND ALL SAMPLE UN SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB	2/89 SAFETY- S IN SECTI EYED= NITS TO BE SEVERITY LOW MEDIUM LOW	PCI= 42 DRAINAGI ON= SURVEYEI SAMPLE ST QUANT 2 5 4	RATE-	SLAB LE SLAB WI NUMBER FING= FAIR SHOULDERS- 20 SLABS DENSITY-PCT 10.00 25.00 20.00	CNGTH - 25.0 LF CDTH - 25.0 LF OF SLABS - 97 OVERALL- 5 2 SAMPLE PCI- 44 DEDUCT-VALUE 8.5 31.8 26.5
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVER RECOMMEND ALL SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR	2/89 SAFETY- S IN SECTI EYED= NITS TO BE SEVERITY LOW MEDIUM LOW N/A	PCI= 42 DRAINAGION= SURVEYEI SAMPLE S QUANT: 2 5 4	RA' E- IZE- ITY SLABS SLABS SLABS	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS DENSITY-PCT 10.00 25.00 20.00	CNGTH - 25.0 LF CDTH - 25.0 LF OF SLABS - 97 OVERALL- 5 2 SAMPLE PCI - 44 DEDUCT-VALUE 8.5 31.8 26.5
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- STOTAL NUMBER OF SAMPLES SURVER NUMBER OF SAMPLES SURVER RECOMMEND ALL SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH	2/89 SAFETY- S IN SECTI EYED- NITS TO BE SEVERITY LOW MEDIUM LOW N/A LOW	PCI= 42 DRAINAGION= SURVEYEI SAMPLE S: 4 14 3	RA' E- IZE- ITY SLABS SLABS SLABS SLABS SLABS	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS DENSITY-PCT 10.00 25.00 20.00 70.00 15.00	OVERALL- SAMPLE PCI- 44 DEDUCT-VALUE 8.5 31.8 26.5 10.7 1.6
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVER RECOMMEND ALL SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH SAMPLE UNIT-5 (RANDOM)	2/89 SAFETY- S IN SECTI EYED= NITS TO BE SEVERITY LOW MEDIUM LOW N/A LOW	PCI= 42 DRAINAGION= SURVEYEI SAMPLE S: 4 14 3 SAMPLE S:	RA' E- IZE- ITY SLABS SLABS SLABS SLABS SLABS	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS DENSITY-PCT 10.00 25.00 20.00 70.00 15.00	OVERALL- SAMPLE PCI- 44 DEDUCT-VALUE 8.5 31.8 26.5 10.7 1.6
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVER RECOMMEND ALL SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH SAMPLE UNIT-5 (RANDOM)	2/89 SAFETY- S IN SECTI EYED= NITS TO BE SEVERITY LOW MEDIUM LOW N/A LOW	PCI= 42 DRAINAGION= SURVEYEI SAMPLE S: 4 14 3 SAMPLE S:	RA' E- IZE- ITY SLABS SLABS SLABS SLABS SLABS	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS DENSITY-PCT 10.00 25.00 20.00 70.00 15.00	OVERALL- SAMPLE PCI- 44 DEDUCT-VALUE 8.5 31.8 26.5 10.7 1.6
SECTION NUMBER - 1 INSPECTION DATE - 06/02 CONDITION- RIDING- 2 TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVER RECOMMEND ALL SAMPLE UNIT-2 (RANDOM) DISTRESS TYPE 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH	2/89 SAFETY- S IN SECTI EYED= NITS TO BE SEVERITY LOW MEDIUM LOW N/A LOW	PCI= 42 DRAINAGION= SURVEYEI SAMPLE S: 4 14 3 SAMPLE S:	RA' E- IZE- ITY SLABS SLABS SLABS SLABS SLABS	SLAB LE SLAB WI NUMBER FAIR SHOULDERS- 20 SLABS DENSITY-PCT 10.00 25.00 20.00 70.00 15.00	OVERALL- SAMPLE PCI- 44 DEDUCT-VALUE 8.5 31.8 26.5 10.7 1.6

63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR	LOW MEDIUM LOW N/A	9 1 2 15	SLABS SLABS SLABS SLABS	47.36 5.26 10.52	19.9 12.0 18.2
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SEC	CION-		
DISTRESS TYPE 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH *** PERCENT OF D. LOAD	LOW LOW HIGH LOW MEDIUM LOW N/A LOW EDUCT VALUES RELATED DIS	47 10 2 27 15 15 72 7 8 BASED (STRESSES	SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS - 78	48.45 10.30 2.06 27.83 15.46 15.46 74.22 7.21 FRESS MECHANI	2.0 6.1 8.1 16.3 24.4 22.7 11.1 0.9
CLIMATE/DURABILITY OTHER					
OTHER	KELAIED DIS	IKESSES	IJ	.70 FERGERI I	DEDUCI VALUES.
BRANCH NAME - WASHR BRANCH NUMBER - A27 SECTION NUMBER - 1	ACK AND ACCE	ESS		SLAB LI SLAB W	ENGTH - 12.5 LF
TYON DOMESTIC TO A MINING THE COLUMN TO A MINING THE COLUMN THE CO				NOIDER	
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPL NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	D2/89 P SAFETY- ES IN SECTION VEYED- D BE SURVEYE PCI BETWEEN	PCI= 92 DRAINAGI DN= CD= I RANDOM	RATE- S	ring= exceli shoulders- surveyed=	LENT OVERALL- 6 2 6
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	D2/89 P SAFETY- ES IN SECTION VEYED- D BE SURVEYE PCI BETWEEN	PCI= 92 DRAINAGI N= CD= I RANDOM	RAZ	FING= EXCEL	LENT OVERALL- 6 2 6 2 134
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-2 (RANDO)	D2/89 P SAFETY- ES IN SECTION VEYED- D BE SURVEYE PCI BETWEEN M) S	PCI= 92 DRAINAGI D= I RANDOM	RATE- S	FING= EXCELSHOULDERS- SURVEYED= 20 SLABS	LENT OVERALL- 6 2 6 2.1 SAMPLE PCI- 90
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-2 (RANDO) DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 73 SHRINKAGE CR	D2/89 P SAFETY- ES IN SECTION VEYED- D BE SURVEYE PCI BETWEEN M) S SEVERITY LOW MEDIUM N/A	CI= 92 DRAINAGI N= I RANDOM AMPLE SI QUANTI 3 20 1	RATE- S UNITS IZE- ITY SLABS SLABS SLABS	SURVEYED= 20 SLABS DENSITY-PCT 15.00 100.00 5.00	LENT OVERALL- 6 2 6 2.1 SAMPLE PCI- 90 DEDUCT-VALUE 5.5 7.0 1.0
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-2 (RANDO) DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 73 SHRINKAGE CR SAMPLE UNIT-4 (RANDO) DISTRESS TYPE 65 JT SEAL DAMAGE	D2/89 P SAFETY- ES IN SECTION VEYED- D BE SURVEYE PCI BETWEEN M) S SEVERITY LOW MEDIUM N/A M) S SEVERITY MEDIUM	PCI= 92 DRAINAGI ON= I RANDOM AMPLE SI QUANTI 3 20 1 GAMPLE SI GAMPLE SI	RATE- STABS SLABS SLABS SLABS SLABS SLABS	SURVEYED= 20 SLABS DENSITY-PCT 15.00 100.00 5.00 16 SLABS DENSITY-PCT 100.00	DEDUCT-VALUE SAMPLE PCI- 93 DEDUCT-VALUE 5.5 7.0 1.0
INSPECTION DATE - 06/CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SUR RECOMMENDED SAMPLES TO STANDARD DEVIATION OF SAMPLE UNIT-2 (RANDO) DISTRESS TYPE 75 CORNER SPALLING 65 JT SEAL DAMAGE 73 SHRINKAGE CR SAMPLE UNIT-4 (RANDO) DISTRESS TYPE	SEVERITY MEDIUM SEVERITY SEVERITY MODEL SERVERITY MODEL SEVERITY MEDIUM MEDIUM	PCI 92 DRAINAGI ON 1 RANDOM AMPLE SI QUANTI AMPLE SI CAMPLE SI QUANTI 16	RATE- UNITS IZE- ITY SLABS SLABS SLABS IZE- ITY SLABS	SURVEYED= 20 SLABS DENSITY-PCT 15.00 100.00 5.00 16 SLABS DENSITY-PCT 100.00	DEDUCT-VALUE SAMPLE PCI- 93 DEDUCT-VALUE 5.5 7.0 1.0

73 SHRINKAGE CR N/A 4 SLABS 2.98 0.8

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES - .00 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 64.81 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES = 35.19 PERCENT DEDUCT VALUES. BRANCH NAME - POWER CHECK PAD SLAB LENGTH - 25.0 LF BRANCH NUMBER - A28B SLAB WIDTH - 20.0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 34 ______ INSPECTION DATE - 06/02/89 PCI= 56 RATING= GOOD CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS-OVERALL-TOTAL NUMBER OF SAMPLES IN SECTION-2 NUMBER OF SAMPLES SURVEYED= 1 RECOMMEND ALL SAMPLE UNITS TO BE SURVEYED. ______ SAMPLE SIZE- 20 SLABS SAMPLE PCI- 56 SAMPLE UNIT-2 (RANDOM)
 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 62 CORNER BR
 LOW
 1 SLABS
 5.00
 4.0

 65 JT SEAL DAMAGE
 HIGH
 20 SLABS
 100.00
 12.0

 67 LG PATCH/UTIL
 LOW
 4 SLABS
 20.00
 10.0

 63 LINEAR CR
 LOW
 17 SLABS
 85.00
 22.0

 63 LINEAR CR
 MEDIUM
 1 SLABS
 5.00
 11.6

 73 SHRINKAGE CR
 N/A
 3 SLABS
 15.00
 2.1

 66 SMALL PATCH
 LOW
 2 SLABS
 10.00
 1.1
 EXTRAPOLATED DISTRESS QUANTITIES FOR SECTION-
 DISTRESS TYPE
 SEVERITY
 QUANTITY
 DENSITY-PCT
 DEDUCT-VALUE

 62 CORNER BR
 LOW
 2 SLABS
 5.88
 4.8

 65 JT SEAL DAMAGE
 HIGH
 34 SLABS
 100.00
 12.0

 67 LG PATCH/UTIL
 LOW
 7 SLABS
 20.58
 10.2

 63 LINEAR CR
 LOW
 29 SLABS
 85.29
 22.0

 63 LINEAR CR
 MEDIUM
 2 SLABS
 5.88
 13.0

 73 SHRINKAGE CR
 N/A
 5 SLABS
 14.70
 2.0

 66 SMALL PATCH
 LOW
 3 SLABS
 8.82
 1.0
 *** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM *** LOAD RELATED DISTRESSES - 61.23 PERCENT DEDUCT VALUES. CLIMATE/DURABILITY RELATED DISTRESSES = 18.46 PERCENT DEDUCT VALUES. OTHER RELATED DISTRESSES - 20.31 PERCENT DEDUCT VALUES. BRANCH NAME - POWER CHECK PAD SLAB LENGTH - 25.0 LF SLAB WIDTH - 25.0 LF NUMBER OF SLABS - 22 BRANCH NUMBER - A29B SECTION NUMBER - 1 ------INSPECTION DATE - 06/02/89 PCI= 68 RATING= GOOD

CONDITION- RIDING- SAFETY- DRAINAGE- SHOULDERS- OVERALL-

_,				
EXTRAPOLATED DISTRESS	QUANTITIES	FOR SECTION-		
NICTORCE TYPE	CEUEDITTV	OUANTTV	DEMOTTY DOT	DEDITOR VALUE
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 67 LG PATCH/UTIL 63 LINEAR CR 66 SMALL PATCH	PEAEVITI	QUANTITI	DENSIII-FCI	DEDUCI-VALUE
/4 JUINI SEALLING	TOM	20 GIVES	100.00	12.0
65 JI SEAL DAMAGE	urgu	JU SLADS	50.00	12.0
67 IC DATED CITE	TOM	COUTS CT	50.00	10.0
67 LG PAIGH/UILL	MEDIUM	2 STADS	6.66	£3.3
66 CMAIT DATCH	TOM	2 SLADS	20.00	2.2
00 STALL FATOR	LOW	0 SLADS	20.00	2.2
*** PERCENT OF DE	DUCT VALUE	S BASED ON DIST	ress mechani	SM ***
LOAD				
CLIMATE/DURABILITY				
OTHER	RELATED DI	STRESSES = 66	.48 PERCENT D	EDUCT VALUES.
DDANGU MAME COUNT	DAME		GIAD IE	NOME TO STE
BRANCH NUMBER - A31E	}		SLAB WI	DTH - 20.0 LF
BRANCH NAME - SOUTH BRANCH NUMBER - A31E SECTION NUMBER - 1			NUMBER	OF SLABS - 854
THE DECEMENT OF THE OF THE	10 /00	DOT 21 DAG	TING DOOD	_ ~ - ~
INSPECTION DATE - 06/0 CONDITION- RIDING-	04 DEMY	PDATNAGE KA.		
				OVERALL- 45 5
NUMBER OF CAMPLES CURY	RAED - 12 IN SECTI	JIΛ==		43
TOTAL NUMBER OF SAMPLES NUMBER OF SAMPLES SURVERECOMMENDED SAMPLES TO	'EIEU= N DE CHDWEV	rn_		18
STANDARD DEVIATION OF	DE SUKVEI	UPANDOM HINTER	CHDUEVED	0 5
SIMPLAND DEVIATION OF				
SAMPLE UNIT-14 (RANDOM				
DISTRESS TYPE	SEVERITY	OUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING	HIGH	1 SLABS	5.00	13.2
65 JT SEAL DAMAGE	HIGH	20 SLABS	100.00	12.0
63 LINEAR CR	HIGH	1 SLABS	5.00	16.2
63 LINEAR CR	LOW	12 SLABS	60.00	21.2
63 LINEAR CR	MEDIUM	5 SLABS	25.00	31.8
70 SCALING/CRAZING	HIGH	1 SLABS	5.00	14.5
/// SCALING/CRAZING	1 (13a)	3 SIAKS	15.00	5.6
72 SHATTERED SLAB	LOW	1 SLABS	5.00	10.9
73 SHRINKAGE CR	N/A	15 SLABS	75 00	11.2
66 SMALL PATCH	HIGH	1 SLABS	5.00	6.3
72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH 66 SMALL PATCH	LOW	4 SLABS	5.00 20.00	2.2
SAMPLE UNIT-30 (RANDOM			16 SLABS	SAMPLE PCI- 41
DISTRESS TYPE			DENSITY-PCT	
65 JT SEAL DAMAGE				
63 LINEAR CR				
63 LINEAR CR				
63 LINEAR CR		3 SLABS		
73 SHRINKAGE CR	N/A	10 SLABS	62.50	9.5

CAMPUR TRATE OF (RANDOM)		WRITE CTOP	00 87 488	CAMPLE DOT 26
SAMPLE UNIT-35 (RANDOM)				
DISTRESS TYPE 74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING	HIGH	1 SLABS	5.00	13.2
65 JT SEAL DAMAGE	MEDIUM	20 SLABS	100.00	7.0
63 LINEAR CR	HIGH	2 SLABS	10.00	26.0
63 LINEAR CR	LOW	11 SLABS	55.00	21.0
63 LINEAR CR	MEDIUM	3 STARS	15.00	24.0
73 SHRINKAGE CR	N/A	11 SLABS	55.00	8.0
SAMPLE UNIT-4 (RANDOM)			20 SLABS	SAMPLE PCI- 21
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING	HIGH	1 SLABS	5.00	13.2
65 JT SEAL DAMAGE	HIGH	20 SLABS	100.00	12.0
63 LINEAR CR	HIGH	1 SLABS	5.00	16.2
63 LINEAR CR	LOW	14 SLABS	70.00	22.0
63 LINEAR CR	MEDIUM	2 SLABS	10.00	19.0
70 SCALING/CRAZING	HIGH	3 SLABS	15.00	34.0
72 SHATTERED SLAB	LOW	1 SLABS	5.00	10.9
74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR 63 LINEAR CR 70 SCALING/CRAZING 72 SHATTERED SLAB 73 SHRINKAGE CR 66 SMALL PATCH	N/A	17 SLABS	85.00	12.7
66 SMALL PATCH	LOW	5 SLABS	25.00	3.2
SAMPLE UNIT-40 (RANDOM)	SA	AMPLE SIZE-	20 SLABS	SAMPLE PCI- 35
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING 74 JOINT SPALLING	HIGH	2 SLABS	10.00	20.8
74 JOINT SPALLING 65 JT SEAL DAMAGE 63 LINEAR CR 63 LINEAR CR	LOW	1 SLABS	5.00	2.2
65 JT SEAL DAMAGE	LOW	20 SLABS	100.00	2.0
63 LINEAR CR	HIGH	1 SLABS	5.00	16.2
63 LINEAR CR	LOW	14 SLABS	70.00	22.0
63 LINEAR CR 73 SHRINKAGE CR	MEDIUM	4 SLABS	20.00	28.2
73 SHRINKAGE CR	N/A	10 SLABS	50.00	/.4
EXTRAPOLATED DISTRESS Q	UANTITIES I	FOR SECTION-		
DISTRESS TYPE	SEVERITY	QUANTITY	DENSITY-PCT	DEDUCT-VALUE
74 JOINT SPALLING	HIGH	45 SLABS	5,26	13.7
74 JOINT SPALLING	LOW	9 SLABS	1.05	0.6
65 JT SEAL DAMAGE	HIGH	498 SLABS	58.31	12.0
65 JT SEAL DAMAGE	LOW	178 SLABS	20.84	2.0
65 JT SEAL DAMAGE	MEDIUM	178 SLABS	20.84	7.0
63 LINEAR CR	HIGH	53 SLABS	6.20	18.8
63 LINEAR CR	LOW	525 SLABS	61.47	21.3
63 LINEAR CR	MEDIUM	151 SLABS	17.68	26.3
70 SCALING/CRAZING	HIGH	36 SLABS	4.21	12.5
70 SCALING/CRAZING	LOW	27 SLABS	3.16	1.4
72 SHATTERED SLAB	LOW	18 SLABS	2.10	5.2
73 SHRINKAGE CR	N/A	561 SLABS	65.69	10.0
66 SMALL PATCH	HIGH	9 SLABS	1.05	1.8
66 SMALL PATCH	LOW	80 SLABS	9.36	1.0

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

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LOAD CLIMATE/DURABILITY OTHER	RELATED DI RELATED DI RELATED DI	STRESSES STRESSES STRESSES	= 53. = 15. = 30.	.59 PERCENT D .72 PERCENT D .69 PERCENT D	EDUCT VALUES. EDUCT VALUES. EDUCT VALUES.	
BRANCH NAME - MAIN OPER APRON ROADWAY SLAB LENGTH - 15.0 LF BRANCH NUMBER - ROAD SLAB WIDTH - 20.0 LF SECTION NUMBER - 1 NUMBER OF SLABS - 354						
INSPECTION DATE - 06/0 CONDITION- RIDING- TOTAL NUMBER OF SAMPLE NUMBER OF SAMPLES SURV RECOMMENDED SAMPLES TO STANDARD DEVIATION OF	2/89 SAFETY- S IN SECTI EYED= BE SURVEY PCI BETWEE	PCI= 74 DRAINAGI ON= ED= N RANDOM	RATE- S	FING= VERY G SHOULDERS - SURVEYED=	OOD OVERALL- 18 3 13 7.5	
SAMPLE UNIT-15 (RANDOM						
DISTRESS TYPE 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE 67 LG PATCH/UTIL 66 SMALL PATCH 66 SMALL PATCH	SEVERITY LOW MEDIUM LOW HIGH MEDIUM LOW MEDIUM	QUANT: 2 2 7 20 1 1 3	SLABS SLABS SLABS SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 10.00 10.00 35.00 100.00 5.00 5.00 15.00	DEDUCT-VALUE 3.7 7.1 8.8 12.0 11.0 0.6 7.6	
SAMPLE UNIT-2 (RANDOM						
DISTRESS TYPE 62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE	SEVERITY LOW LOW MEDIUM LOW HIGH	QUANT: 1 2 1 2 20	ITY SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 5.00 10.00 5.00 10.00 100.00	DEDUCT-VALUE 4.0 3.7 3.3 3.5 12.0	
SAMPLE UNIT-9 (RANDOM)	SAMPLE S	IZE-	20 SLABS	SAMPLE PCI- 81	
DISTRESS TYPE 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE	LOW LOW HIGH	2 2 20	SLABS SLABS SLABS	10.00 10.00 100.00	3.7 3.5	
EXTRAPOLATED DISTRESS	•					
DISTRESS TYPE 62 CORNER BR 75 CORNER SPALLING 75 CORNER SPALLING 74 JOINT SPALLING 65 JT SEAL DAMAGE	SEVERITY LOW LOW MEDIUM LOW HIGH	QUANT: 6 35 18 65 354	SLABS SLABS SLABS SLABS SLABS SLABS	DENSITY-PCT 1.69 9.88 5.08 18.36 100.00	DEDUCT-VALUE 1.2 3.6 3.4 5.5 12.0	

67 LG PATCH/UTIL MEDIUM 6 SLABS 1.69 4.2 66 SMALL PATCH LOW 6 SLABS 1.69 0.2 66 SMALL PATCH MEDIUM 18 SLABS 5.08 2.7

*** PERCENT OF DEDUCT VALUES BASED ON DISTRESS MECHANISM ***

LOAD RELATED DISTRESSES = 3.66 PERCENT DEDUCT VALUES.

CLIMATE/DURABILITY RELATED DISTRESSES = 36.59 PERCENT DEDUCT VALUES.

OTHER RELATED DISTRESSES = 59.76 PERCENT DEDUCT VALUES.